

Ministry of Transportation (MTO)

Northwestern Region

Design and Construction Report

Detail Design and Class Environmental Assessment for Provincial Transportation
Facilities, (2000)

Group 'B' Process

Highway 17 Curve Realignment and Hewitson Creek Structural Culvert Replacement

Highway 17, 10.6 km west of Schreiber
GWP 6333-14-00

**HIGHWAY 17 CURVE REALIGNMENT, HEWITSON CREEK CULVERT
REPLACEMENT
10.6 KM WEST OF SCHREIBER
DETAIL DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT (GROUP B)
MTO FILE: GWP 6333-14-00
DESIGN AND CONSTRUCTION REPORT**

Prepared for:

Ministry of Transportation
Project Delivery, Northwestern Region
Thunder Bay, ON

Prepared by:

Hatch Limited
973 Balmoral Street
Thunder Bay, ON

Prepared by:



Audrey Nerino, BES.
Environmental Planner
Hatch



Erin McCulloch, B.Sc.
Environmental Planner
Hatch

Reviewed by:



Mark Armstrong, M.E.Sc., P.Eng., PMP
Director, Project Development
Hatch



Dale Wiersema, P.Eng.
Principal Project Manager
Hatch

THE PUBLIC RECORD

This detail design and Class Environmental Assessment (Class EA) is being carried out as a Group 'B' undertaking following the Ministry of Transportation of Ontario (MTO) Class Environmental Assessment for Provincial Transportation Facilities (2000). The planning and preliminary design study has been completed and documented in an approved Transportation Environmental Study Report (TESR). This Design and Construction Report (DCR) documents the detail design study process and how commitments in the TESR and environmental protection measures have been addressed.

This DCR is available for public comment from Tuesday, November 12, 2024 to Friday December 13, 2024 on the project website: www.hwy17hewitson.com and at the following locations during normal hours of operation:

Schreiber Municipal Complex
204 Alberta Street
Schreiber, ON
Mon-Fri: 8:30 a.m.- 4:30 p.m.

Schreiber Public Library
314 Scotia Street
Schreiber, ON
Tues-Sat: 1 p.m. - 5 p.m.

Waverley Resource Library
285 Red River Road
Thunder Bay, ON
Tues: 9:30 a.m. - 8 p.m.
Wed-Sat: 9:30 a.m. - 5 p.m.

Interested persons are encouraged to review the document and provide comments to the following project team contacts as listed below **by Friday, December 13, 2024:**

Patrick Van Belleghem, P.Eng
Project Engineer – Project Delivery
Ministry of Transportation
615 James Street South
Thunder Bay, ON P7E 6P6
tel: 807-252-9757, toll free: 1-800-465-5034
fax: 807-473-2168
e-mail: patrick.vanbelleghem@ontario.ca

Dale Wiersema, P.Eng
Principal Project Manager
Hatch Limited
973 Balmoral Street, Suite 101
Thunder Bay, ON P7B 0E2
tel: 807-625-8728
fax: 807-623-5925
e-mail: dale.wiersema@hatch.com

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, will become part of the public record. If you have any accessibility requirements in order to participate in this project, please contact one of the Project Team members listed above.

Ce document hautement spécialisé n'est disponible qu'en anglais en vertu du règlement 411/97, qui en exempte l'application de la Loi sur les services en français. Pour de l'aide en français, veuillez communiquer avec le ministère des Transports, Bureau des services en français au: 905-704-2045 ou 905-704-2046.

Table of Contents

1. Overview	1
1.1 Summary Description of the Undertaking.....	1
1.2 Purpose of the DCR	2
1.3 Regulatory Framework	3
1.3.1 Environmental Assessment Act.....	3
1.3.2 Ministry of Transportation Class Environmental Assessment Process	3
1.3.3 Provincial Parks and Conservation Reserves Environmental Assessment Process.....	4
1.4 Project Location.....	4
1.4.1 Provincial Setting	4
1.4.2 Regional Setting	5
2. Consultation Process	5
2.1 Consultation during Preliminary Design	5
2.2 Post-TESR Consultation.....	6
2.3 Consultation during Detail Design.....	6
2.4 Notice of Commencement of Detail Design	6
2.4.1 Agencies.....	6
2.4.2 Indigenous Communities and Organizations	8
2.4.3 Public / Property Owners	9
2.4.4 Newspaper Notice.....	9
2.5 Stakeholder Meeting.....	10
2.6 Notice of Completion of DCR.....	10
3. Review of Approved TESR Commitments.....	10
3.1 Changes to Recommended Plan	10
3.2 Changes to Commitments	11
4. Detailed Description of the Recommended Plan	11
4.1 Major Features of the Proposed Work	11
4.1.1 Highway Realignment	11
4.1.2 Hewitson Creek Culvert	13
4.1.3 Non-Structural Culverts.....	14
4.1.4 Rainbow Falls Provincial Park.....	15
4.1.5 Decommissioning of Existing MTO Right-of-Way	16
4.1.6 Entrances	16
4.1.7 Casque Isles Hiking Trail	17
4.1.8 Vegetation Clearing	18
4.1.9 Roadside Safety Improvements	18
4.1.10 Utilities	18
4.1.11 Construction Staging.....	18
5. Environmental Issues and Commitments	18
5.1 Surface / Groundwater.....	18

5.1.1	Mitigation	19
5.2	Archaeology	19
5.2.1	Mitigation	20
5.3	Built Landscapes and Cultural Heritage Features	20
5.4	Memorial Cross	20
5.4.1	Mitigation	20
5.5	Fish and Fish Habitat	20
5.5.1	Mitigation	22
5.6	Terrestrial Ecosystem	23
5.6.1	Vegetation and Natural Heritage Values	23
5.6.2	Wildlife	23
5.6.3	Species at Risk	24
5.6.4	Mitigation	24
5.7	Noise	25
5.7.1	Mitigation	25
5.8	Waste and Surplus Earth Materials	25
5.8.1	Mitigation	25
5.9	Emergency Spill Response	26
5.9.1	Mitigation	26
6.	Monitoring	26
6.1	Environmental Protection during Construction	26
7.	Summary of Environmental Concerns and Commitments	27
8.	Reference Documents	33

List of Figures

Figure 1.	Location Map	1
Figure 2.	Inlet of Hewitson Creek culvert (July 2024).	2
Figure 3.	Outlet of Hewitson Creek culvert (July 2024).....	2
Figure 4.	Provincial setting	4
Figure 5.	Regional setting	5
Figure 6.	Highway 17 Realignment Plan	12
Figure 7.	New Hewitson Creek Culvert Cross Section.....	13
Figure 8.	Existing Hewitson Creek Culvert Removal Cross Section	13
Figure 9.	Waterbodies within the study area	14
Figure 10.	Cross Section of Restored Channel at Unnamed Watercourse	15
Figure 11.	New Parking area at Rainbow Falls Provincial Park	16
Figure 12.	Casque Isles Hiking Trail Realignment.....	17

List of Tables

Table 1.	Agency Correspondence	7
----------	-----------------------------	---

Table 2. Summary of Comments from Public / Property Owners.....	9
Table 3. Summary of Environmental Concerns and Commitments.....	27

List of Appendices

Appendix A

Notices and Mailing Lists

Appendix B

Correspondence

Appendix C

Plans

EXECUTIVE SUMMARY

Introduction

The Ministry of Transportation of Ontario (MTO) has retained Hatch Ltd. to undertake the Detail Design study and Class Environmental Assessment (Class EA) for the realignment of a 3.8 km segment of Highway 17 and the replacement of the Hewitson Creek Culvert along the new highway alignment. The realigned section of Highway 17 will be designed to meet current highway design standards. The Project Area is located approximately 10.6 km west of Schreiber within the District of Thunder Bay, ON.

Environmental Assessment Process

This project is being carried out following the requirements of the Ministry of Transportation's Class Environmental Assessment for Provincial Transportation Facilities (MTO Class EA). The detail design study is proceeding as a Group B project under the MTO Class EA (2000) involving "major changes to existing transportation facilities".

The Class Environmental Assessment was initiated as a Group B project during preliminary design. As the project impacts a portion of Rainbow Falls Provincial Park, meeting obligations under the Provincial Parks and Conservation Reserve Class EA (PPCR EA) were required. These obligations could be addressed under the MTO Class EA process, through a harmonized EA incorporating features from PPCR EA into the MTO EA process. During the preliminary design phase, a Transportation Environmental Study Report (TESR) was completed, and a Notice of Completion was issued which included a 30-day public comment period. During this time, no Part II Orders were made, and the project was deemed to have been approved under the Ontario *Environmental Assessment Act*, able to proceed to detail design. The project continued through detail design as a Group B undertaking under the MTO Class EA. Ontario Parks filed a Statement of Completion for the works subject to the PPCR EA on October 25, 2022.

Consultation Process

Consultation was initiated during the planning and preliminary design phase in 2021 and involved two major points of contact with the opportunity for public comment throughout. The Commencement of Preliminary Design Study Notice and a Notice of Completion of the TESR were issued in August 2020 and October 2021, respectively.

Consultation continued throughout the detail design phase, with a Notice of Commencement of Detail Design Study Notice and a Notice of Completion of Design and Construction Report issued in April 2024 and November 2024, respectively. Both points of contact provided the opportunity for public comments throughout the study.

Selected Alternative

Detail design finalized the preferred alternative that was identified during preliminary design. The alternative that was carried forward in detail design involves the realignment of Highway 17 to the south of the Hydro transmission corridor, a new Hewitson Creek structural culvert along the new alignment, and construction of a new entrance road and parking area at Rainbow Falls Provincial Park.

1. Overview

The **Ontario Ministry of Transportation (MTO)** retained **Hatch Ltd.** to complete the detail design and Class Environmental Assessment process for the realignment of a 3.8 km segment of Highway 17 and the replacement of Hewitson Creek (official name 'Hewitson River') culvert along the new alignment. The Project Area is located 10.6 km west of Schreiber, ON.

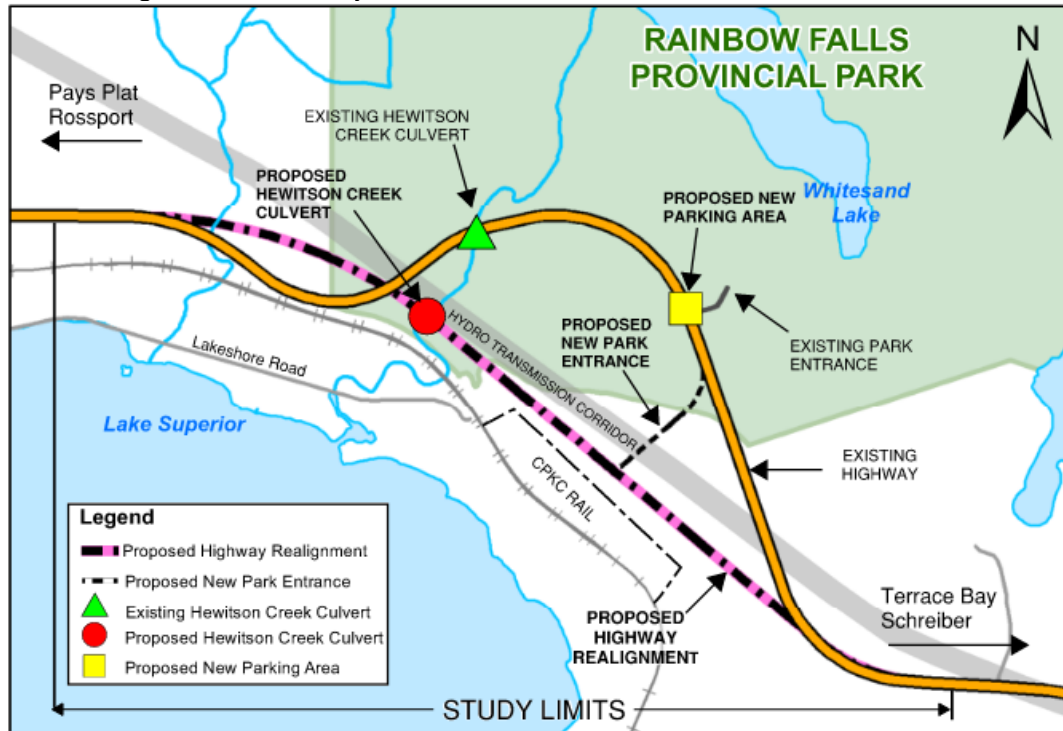


Figure 1. Location Map

1.1 Summary Description of the Undertaking

This section of Highway 17 is the primary route of the TransCanada highway. Highway 17 near Hewitson Creek is generally used as one of the main transportation corridors for long distance travel and transportation of goods through the province. This section of highway was originally constructed in 1933, and the existing horizontal and vertical highway alignments within the study area do not meet current MTO guidelines. The highway within the study area consists of a single lane in each direction as well as an eastbound truck-climbing lane. A portion of the study area intersects segments of Rainbow Falls Provincial Park.

The preliminary design and MTO Class Environmental Assessment was carried out in accordance with the approved environmental planning process for a Group B project under the Ministry of Transportation's Class Environmental Assessment for Provincial Transportation Facilities (2000), which was documented in a Transportation Environmental Study Report (TESR), approved by MECP in 2022.

During preliminary design, a preferred alternative was developed which included the following:

- Realigning 3.8 km of Highway 17 south of the Hydro transmission corridor and north of the Canadian Pacific Kansas City (CPKC) tracks to meet current Ministry design standards.
- Within the realignment, a new structural culvert crossing at Hewitson Creek will be installed.
- Throughout the new alignment, non-structural culvert crossings conveying small watercourses and drainage features will be installed.
- The existing segment of Highway 17 will be decommissioned, including the removal of the existing Hewitson Creek culvert and highway infrastructure (signage, drainage culverts, guiderail etc.).
- A new access road from the realigned highway to Rainbow Falls Provincial Park and a new parking area will be constructed.

The Hewitson Creek concrete barrel arch structure is located on Highway 17, approximately 10.6 km west of Schreiber's west limit. The concrete barrel arch structure was constructed in 1952 and has a span of 9.1 m and a length of 67 m (Figures 2 and 3). It conveys Hewitson Creek (official geographical name 'Hewitson River') south to Lake Superior, with rapids and runs at the outlet of Whitesand Lake to the culvert inlet and steeply sloping natural bedrock drops and rapids downstream of the culvert.



Figure 2. Inlet of Hewitson Creek culvert (July 2024).

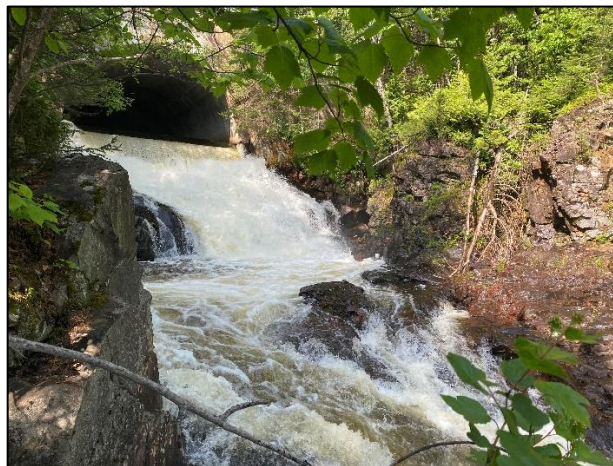


Figure 3. Outlet of Hewitson Creek culvert (July 2024).

1.2 Purpose of the DCR

This Design and Construction Report (DCR) has been prepared to:

- Document how the design related commitments in the approved TESR were met in the detail design (including commitments made to future consultation).
- Document the detail design process (the development of the approved preliminary design to the design implementation level of detail).

- Describe the recommended plan.
- Document anticipated environmental impacts and commitments to mitigation measures.
- Identify project approvals, licenses and permits that have been or must be obtained prior to construction.

1.3 Regulatory Framework

1.3.1 Environmental Assessment Act

The project is subject to the *Ontario Environmental Assessment Act* R.S.O. 1990 (EA Act), which allows for the undertaking of Class Environmental Assessments (Class EAs). Class EAs are approved planning documents defining activities and EA processes required for the undertaking. Class EAs are streamlined self-assessment processes which fulfill the requirements of the EA Act without requiring the submission of an Individual EA to the Minister of Environment, Conservation, and Parks. Formal reviews and approvals under the EA Act are not required provided the approved planning and decision-making processes under a Class EA are followed.

1.3.2 Ministry of Transportation Class Environmental Assessment Process

The MTO Class EA is an approved process under the EA Act for planning and designing provincial highway projects. This study is following an approved planning process for a Group B Project under the Class Environmental Assessment for Provincial Transportation Facilities (2000).

In accordance with the 2000 MTO Class EA, 'Group B' projects are classified as '*major improvements to existing transportation facilities including highway improvements such as major alignment shifts that substantially follow the existing right-of-way.*' In 2021, the MTO completed the preliminary design study and Class EA for a Group B Project to evaluate various alternatives to address the existing deficiencies along Highway 17 within the project area. During the preliminary design, the evaluation of various alternatives identified a preferred alternative, which has been carried forward to detail design. A Transportation Environmental Study Report (TESR) was prepared in 2021 to complete the preliminary design phase and was subject to a public comment period.

The DCR has been prepared to document the detail design study process including a review of the TESR, consisting of how commitments identified in the TESR have been addressed during detail design, and how environmental protection measures are incorporated into the construction project.

Since the improvements identified in this project are consistent with the project described in the approved TESR, an Addendum to the TESR is not required, and there are no further opportunities to request an individual Environmental Assessment (Part II Order).

1.3.3 Provincial Parks and Conservation Reserves Environmental Assessment Process

The Provincial Parks and Conservation Reserves EA Process (PPCR Class EA) is a Class EA approved under the *EA Act* which addresses developments impacting provincial parks resources. As the project will impact part of Rainbow Falls Provincial Park, Ontario Parks (MECP) was consulted with throughout the duration of the preliminary design study to ensure the PPCR Class EA planning processes were captured within the MTO Class EA. Commitments were made in the TESR to continue ongoing consultation with MECP during detail design.

The realignment of the park access road, the establishment of a new parking area, and decommissioning of the existing MTO Right of Way for the eventual integration of lands into the park boundary are components within the park that fall under the application of the PPCR Class EA. Ontario Parks has submitted a Statement of Completion for the PPCR EA process on October 25, 2022. As such, obligations under the PPCR EA have been met.

1.4 Project Location

1.4.1 Provincial Setting

This project is located in Northwestern Ontario, east of the City of Thunder Bay (Figure 4).



Figure 4. Provincial setting

1.4.2 Regional Setting

As illustrated below, the project is located on Highway 17, within the electoral district of Thunder Bay-Superior North, partially within the Rainbow Falls Provincial Park boundary and within the geographic Townships of Lahontan and Killraine. Hewitson Creek culvert is located approximately 10.6 km west of Schreiber within the geographic Township of Killraine.

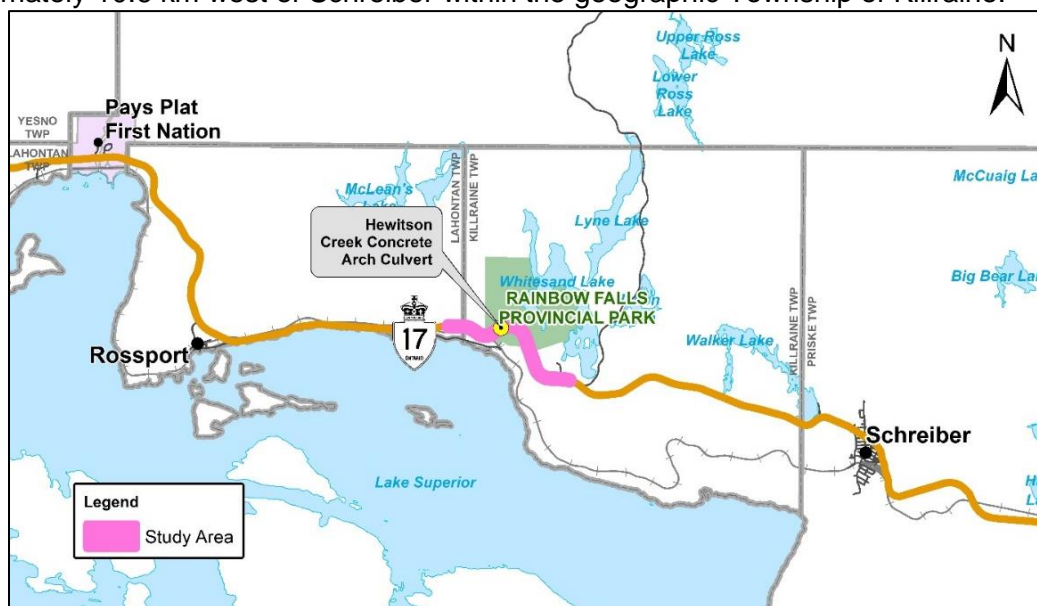


Figure 5. Regional setting

2. Consultation Process

2.1 Consultation during Preliminary Design

During preliminary design, consultation was consistent with the requirements of a Group B project and included the following:

- Notice of Commencement of Study letters issued to agency groups, MECP mandatory contact list, Rainbow Falls Provincial Park business mailing list; Indigenous communities within 100 km of study area; the public including Rainbow Falls Provincial Park private citizens and adjacent property owners on August 24, 2020.
- An Ontario Government Notice issued in the Thunder Bay Chronicle Journal and Terrace Bay-Schreiber Newson August 25, 2020.
- Several meetings with MECP (Ontario Parks).
- A Notice of Completion of TESR issued to the numerous contact lists developed during the study and published in Thunder Bay Chronicle Journal and Terrace Bay-Schreiber News on October 25, 2021.

Throughout the duration of the study, stakeholders, members of the public, residents, and Indigenous communities were encouraged to provide comments and concerns on the project. All comments received and responses provided were documented in the TESR.

2.2 Post-TESR Consultation

In response to the publication of the TESR, comments were received from external agencies and members of the public. The comments received and responses provided, or resolutions developed to address the comments are summarized in a table in Appendix B.

2.3 Consultation during Detail Design

Public consultation for this study followed the process for a 'Group B' project under the MTO Class EA. External consultation for this Group B undertaking included two key points of contact:

1. Notice of Study Commencement
2. Notice of Completion of Design and Construction Report (DCR)

Further details of the public consultation undertaken during the detail design phase are included below.

2.4 Notice of Commencement of Detail Design

A Notice of Commencement outlining the project and study process was issued to applicable stakeholders, including property owners and Indigenous groups in March 2024. The notice of commencement was published in local newspapers in April 2024. Details of notifications are further described below.

2.4.1 Agencies

The mailing list of agencies contacted during preliminary design was reviewed and updated as required. The following agency groups were notified of the Notice of Commencement issued via e-mail or letter-mail on March 29, 2024.

Provincial: Ministry of Northern Development (MND), Ministry of Natural Resources (MNR), Ministry of Environment, Conservation and Parks (MECP), MECP – Ontario Parks (Rainbow Falls Provincial Park), Ministry of Citizenship and Multiculturalism (MCM), Ministry of Heritage, Sport, Tourism, Culture (MHSTC), Ministry of Mines

Municipal: Township of Terrace Bay, Township of Schreiber, Rossport Local Services Board

School Transportation: East of Thunder Bay Transportation Consortium

Emergency Services: Superior North Emergency Services (EMS), Ontario Provincial Police (OPP), Township of Schreiber Fire and Rescue, Terrace Bay Fire and Emergency Services

Stakeholders and Interest Groups: Casque Isles Hiking Club (CIHC), Canadian Pacific Kansas City (CPKC) Railway, Voyageur Trail Association, TransCanada Trail, Filanes Motel, numerous regional / local provincial agencies, municipal businesses, interest groups on Rainbow Falls Provincial Park Management Plan mailing list

MECP-Ontario Parks Contact List: Numerous federal, provincial agencies, tourism, recreational, and interest groups and associations

During the detail design study, comments were received from a total of six agency groups. Table 1 below summarized the correspondence with agencies during the study. Copies of correspondence are included in Appendix B.

Table 1. Agency Correspondence

Contact	Comment	Response / Resolution
OPP Northwest	<i>Comment received March 28, 2024.</i> Acknowledgement of project notice	<i>No response required.</i>
Ministry of Citizenship and Multiculturalism	<i>Comment received April 29, 2024:</i> Attached letter with recommendations to complete built heritage and cultural landscape screening	<i>Response provided May 14, 2024:</i> Screening checklists for built heritage and cultural landscapes for the project area completed by qualified persons in preliminary design. Results of screening to be documented in the Design and Construction Report.
Nature and Outdoor Tourism Ontario (NOTO)	<i>Comment received March 28, 2024:</i> Asked to share notice of study commencement letter in organization newsletter	<i>Response sent April 2, 2024:</i> MTO in support of making notice available in newsletter
MNR Nipigon District	<i>Comment received April 3, 2024:</i> Requested confirmation that all previous correspondence MNR provided relating to the project is on file	<i>Response provided April 17, 2024:</i> Tables and email copies of all existing MNR correspondence on file provided to MNR

Contact	Comment	Response / Resolution
	<p><i>Comment received May 24, 2024:</i></p> <p>Requested confirmation of Parks contacts associated with project and noted that responses to previous MNR comments were outstanding.</p>	<p><i>Responses provided July 24, Oct 15, 2024:</i></p> <p>Ontario Parks contact provided to MNR. Responses to outstanding comments provided by MTO.</p>
Casque Isles Hiking Club (CIHC)	<p><i>Comment received March 28, 2024:</i></p> <p>Ongoing discussions with CIHC, provided kmz of existing trail and proposed temporary trail alignment</p>	<p><i>Comment received May 13, 2024:</i></p> <p>New permanent trail crossing has been established in consultation with CHIC, MTO, and MECP. Trail location selected based on pedestrian and driver safety, impacts to adjacent lands, constructability.</p>
Ministry of Environment, Conservation, and Parks	<p><i>Comment received April 15, 2024:</i></p> <p>Provided attached Project Acknowledgement letter and Proponents Guide to Preliminary Screening of SAR</p>	<p>Response not required. Communication with MECP will be carried out as required to meet criteria under Group B project.</p>

A sample letter and detailed mailing list of the agencies contacted is included in Appendix A.

2.4.2 Indigenous Communities and Organizations

A Commencement of Study Notice was distributed by MTO to the following Indigenous groups within proximity to the project area by MTO the week of April 8, 2024.

- Pays Plat First Nation
- Biigtigong Nishnaabeg
- Red Rock Indian Band
- Biinjitiwaabik Zaaging Anishinaabek
- Ginoogaming First Nation
- Long Lake #58 First Nation
- Red Sky Métis Independent Nation
- Métis Nation of Ontario
- Jackfish Métis Association

During the study, no comments were received from Indigenous groups in response to the notice of study. However, MTO carried out consultation with Indigenous communities throughout the duration of the study.

2.4.3 Public / Property Owners

Members of the public and adjacent property owners contacted during preliminary design received the Notice of Commencement by letter mail or email on March 29, 2024. An updated Last Registered Owner (LRO) title search was provided by MTO. Further, the Notice of Commencement was distributed to private citizen contacts provided by Rainbow Falls Provincial Park. During the study, one comment was received from a nearby property owner, correspondence is summarized below:

Table 2. Summary of Comments from Public / Property Owners

Contact	Comment	Response / Resolution
Property Owner	<i>Comment received April 29, 2024</i> While turning east on Hwy 17 from Hunter Road, traffic suddenly appears from a low section on road	Sight distances from Hunter Road were reviewed and were found not to follow current Ministry guidelines. MTO will conduct all necessary consultation, finalize design and road closure requirements to potentially re-establish access for Hunter Road at the Winston / Whitesand Lake Road. If feasible, this will be included into the contract; the Contractor will provide residents with advanced notice prior to any changes to access occurring.

2.4.4 Newspaper Notice

The Notice of Study Commencement was published in the following local newspapers on Tuesday, April 2, 2024:

- The Chronicle Journal
- Terrace Bay / Schreiber News

A copy of the newspaper publication is included in Appendix A.

2.5 Stakeholder Meeting

Throughout the detail design study, MTO held in-person and virtual meetings with the following groups:

- Meeting #1 with MECP-Ontario Parks – April 2024
- Meeting with Casque Isles Hiking Club – August 2024
- Meeting #2 with MECP - Ontario Parks – October 2024
- Meeting with MNR Nipigon District and MECP – October 2024

2.6 Notice of Completion of DCR

To commence the public comment period of the DCR, a Notice of Completion of the DCR was distributed to the same contacts who received the Notice of Study, updated as required on Friday November 8, 2024, through letter mail and on Monday November 11, 2024 through email.

The Notice of Completion of DCR was also published in the following local newspapers on Tuesday, November 12, 2024:

- Thunder Bay Chronicle Journal
- Terrace Bay / Schreiber News

A project website was created to provide a copy of the DCR online for public comment at <https://www.hwy17hewitson.com>. The website includes details of the project, Class EA process, and the opportunity to contact the project team.

This DCR is available for a 30-day public comment period and interested persons are encouraged to review the DCR and provide comments within the 30 days. If there are no outstanding concerns after the 30-day comment period has expired, further documentation will not be prepared, and construction may commence without further notice.

3. Review of Approved TESR Commitments

3.1 Changes to Recommended Plan

There have been no significant changes to the recommended plan identified in the TESR. The overall concept has been further developed and refined to a level appropriate for construction. Since approval of the TESR, the scope of work has been modified to include the installation of fully paved shoulders along Highway 17 west of the project limits and the widening of the east shoulder along the new park access road to accommodate hikers. The project team is reviewing the potential closure and re-establishment of Hunter Road at the east project limits to provide a safer entrance. MTO will conduct all necessary consultation prior to finalizing the design and road closure requirements to potentially re-establish access for Hunter Road at the Winston/Whitesand Lake Road. If feasible, this will be included in the contract; the Contractor will provide residents with advanced notice prior to any changes to access occurring.

3.2 Changes to Commitments

The commitments outlined in the TESR have been carried forward into detail design and have been reviewed to ensure the study addressed each commitment. The DCR illustrates how the commitments made regarding consultation, future studies, and design considerations were addressed during this detail design study. The Summary of Environmental Concerns and Commitments Table (Table 3) identifies all relevant mitigation measures that will be implemented to address.

4. Detailed Description of the Recommended Plan

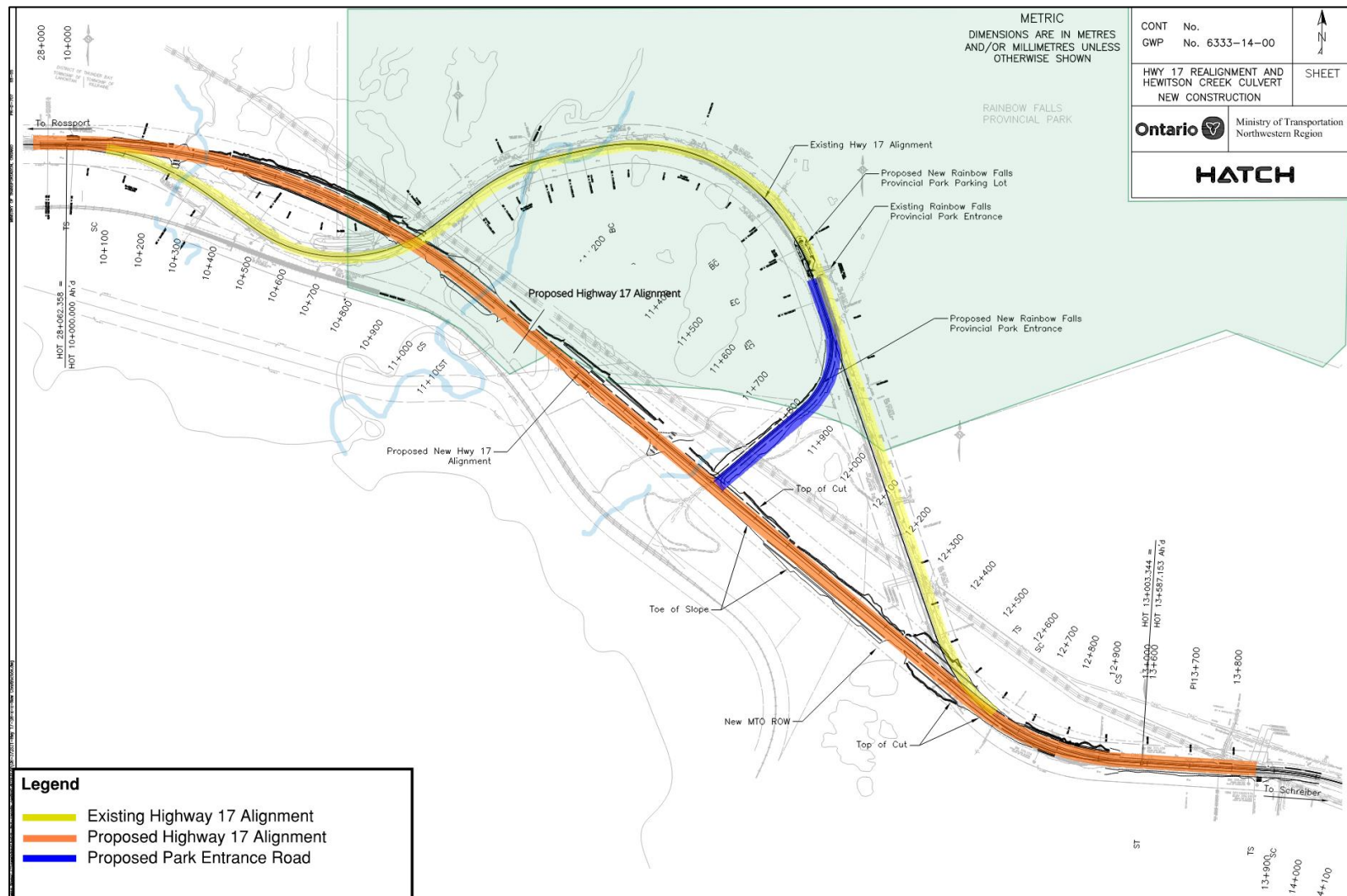
4.1 Major Features of the Proposed Work

Major features of the recommended plan include:

- Highway realignment
- Hewitson Creek culvert
- Non-structural culverts
- Rainbow Falls Provincial Park entrance road and parking lot
- Decommissioning of existing alignment
- Entrances
- Casque Isle Hiking Club
- Vegetation clearing
- Roadside safety
- Utility relocations
- Construction staging

4.1.1 Highway Realignment

To address the existing substandard horizontal and vertical alignment within the project area, the 3.8 km segment of Highway 17 will be realigned. The selected realignment north of the CPKC tracks and south of the Hydro Transmission corridor has been carried forward through detail design based on numerous factors that were evaluated during the study including constructability, property, environmental, and utility impacts. The realignment includes the construction of a new access road into Rainbow Falls Provincial Park. A plan showing the realignment is shown in Figure 6 on the following page.



4.1.2 Hewitson Creek Culvert

A new Hewitson Creek culvert crossing will be installed along the new highway alignment, which consists of a 74.45 m long, 14.6 m wide, 8 m high open bottom single-span precast concrete arch culvert. A typical cross section of the new Hewitson Creek culvert is detailed in Figure 7 below. The structure includes a layer of waterbody materials along the channel bottom and a low flow channel in the center of the culvert. The low flow channel is intended to allow fish passage during periods of lower water levels by concentrating flows to maintain sufficient water depth. The embankments around the culvert will be stabilized with rock protection.

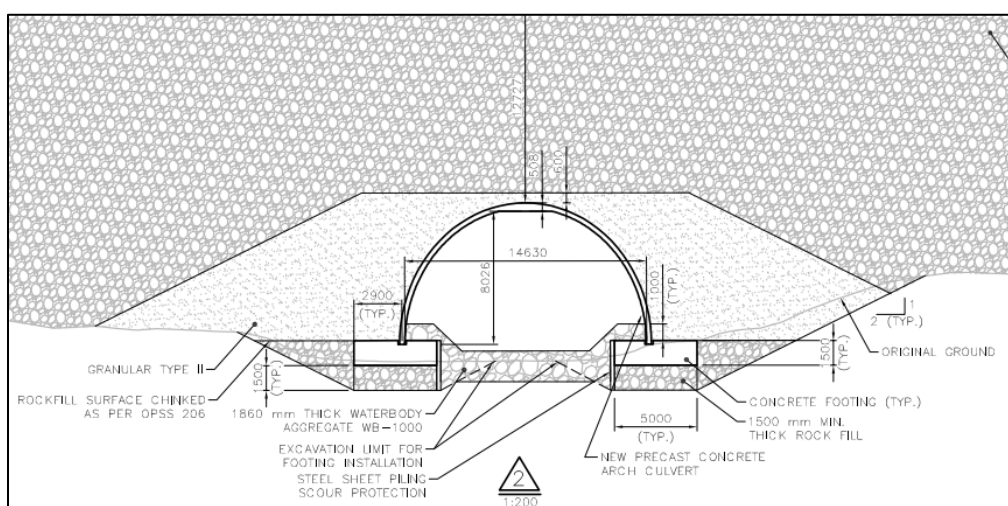


Figure 7. New Hewitson Creek Culvert Cross Section

At the existing Hewitson Creek culvert, located 800 m west of the existing Rainbow Falls Provincial Park entrance (48.8401, -87.4050), the 66.77 m long, 9.5 m wide, 5.18 m high concrete arch culvert will be removed to restore an open channel. The banks will be graded to a stable slope and capped with rock protection. The banks above the rock protection will be capped with surplus earth materials and revegetated with Native Seed Mix, shrubs, and tree seedlings.

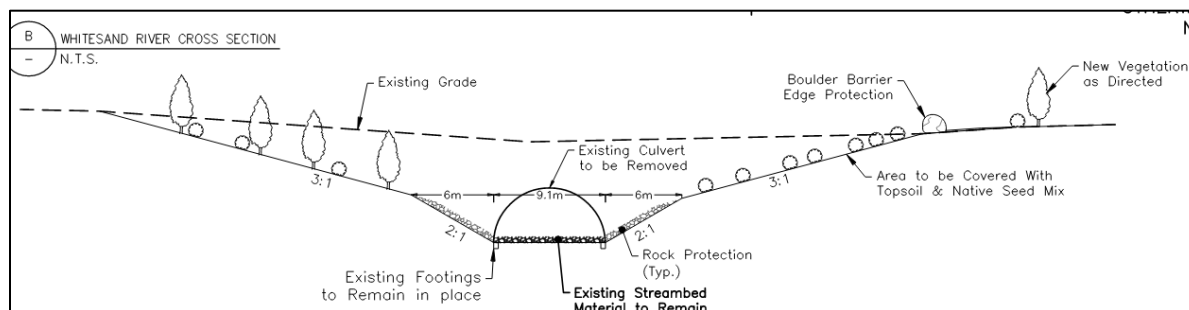


Figure 8. Existing Hewitson Creek Culvert Removal Cross Section

4.1.3 Non-Structural Culverts

4.1.3.1 Fisheries Watercourses

A location map identifying waterbodies within the study area is shown in Figure 9 below. Details of the work proposed at the watercourses are included in the following sections.



Figure 9. Waterbodies within the study area

4.1.3.1.1 Unnamed Watercourse

An unnamed watercourse crosses the existing Highway 17 alignment, approximately 1.8 km west of the Rainbow Falls Provincial Park entrance (48.8400, -87.4164) and is conveyed through a 22.73 m long, 1.9 m wide, 1.3 m high culvert. The culvert along the existing highway alignment will be removed and the natural streambed and banks will be restored. The channel will be constructed to match existing conditions and installed with waterbody material. The banks will be constructed to a stable slope and installed with rock protection. Above the rock protection, the banks will be capped with topsoil. Shrub and tree seedlings will be planted along the banks. A low flow channel will be constructed throughout the waterbody materials.

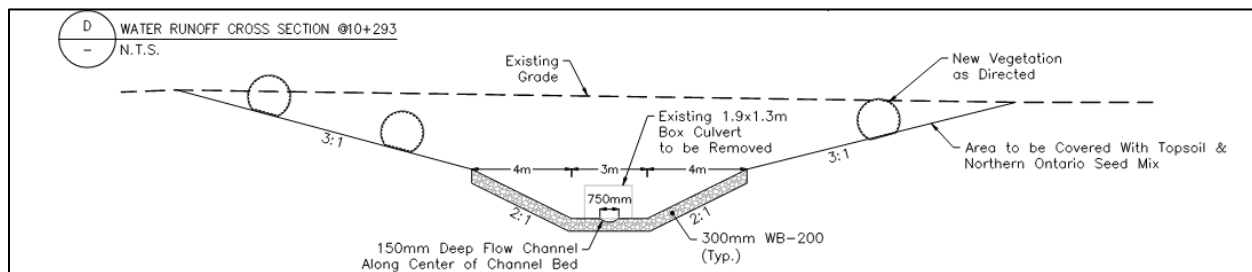


Figure 10. Cross Section of Restored Channel at Unnamed Watercourse

To accommodate the new highway alignment, a new culvert crossing will be installed 50 m upstream (north) of the existing crossing. The bottom of the culvert will be placed below the streambed and will be installed with waterbody material and a low flow channel. Banks will be stabilized with rock protection and planted with seed and shrubs to reflect natural conditions.

4.1.3.1.2 Spring / Tributary to Lake Superior

A natural spring originating south of the existing highway alignment near the existing Rainbow Falls Provincial Park entrance will intersect the new alignment. To convey flow across the highway, a new culvert will be installed. The bottom of the culvert will be placed below the natural channel bed, installed with waterbody materials, and a low flow channel.

4.1.3.1.3 Tributary to Hewitson Creek

A tributary to Hewitson Creek is located approximately 960 m west of the existing Rainbow Falls Provincial Park entrance (48.8394, -87.4068) and is conveyed through a 1.3 m wide concrete box culvert. The culvert will be removed, and a natural channel bed and banks will be constructed and installed with riprap. The channel flows into Hewitson Creek north of the new alignment and a culvert crossing along the new alignment is not required at this tributary.

4.1.3.2 Drainage Improvements

Drainage was assessed throughout the project limits to ensure positive drainage is achieved along the new highway alignment. Three (3) drainage culverts which do not support fish habitat will be removed along the existing alignment, while one existing culvert is maintained. To manage drainage along the new alignment, two new drainage culverts will be installed. Ditching will be completed throughout the project area where required.

4.1.4 Rainbow Falls Provincial Park

4.1.4.1 Park Entrance Road

A new entrance road to Rainbow Falls Provincial Park will be constructed north of the new Highway 17 alignment and has been designed to meet current MTO standards. The new park entrance includes a wide shoulder along the east lane to accommodate pedestrians as well as turning lanes on Highway 17.

4.1.4.2 Parking Area

A new parking lot will be constructed at the existing entrance to Rainbow Falls Provincial Park. The parking lot will consist of spaces for passenger vehicles and for longer trailer campers. The existing gate at the park will remain in its current location and an additional gate will be installed

further south to include the pumping and filling station at the park. A drawing of the parking area is shown below.

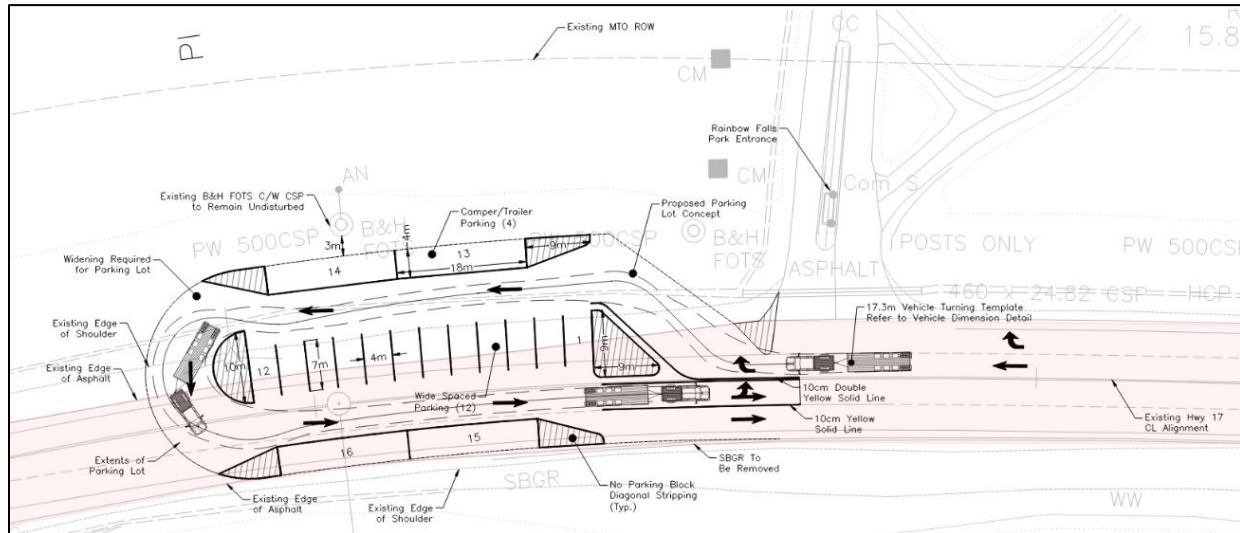


Figure 11. New Parking area at Rainbow Falls Provincial Park

4.1.5 Decommissioning of Existing MTO Right-of-Way

Upon decommissioning of the existing highway alignment, MTO will initiate the transfer of the Right-of-Way to facilitate the designation of park lands (within the park boundary) or as Crown land (outside of the park boundary). The existing alignment will be decommissioned to support this transfer and the potential future development of a recreational trail within Rainbow Falls Provincial Park.

As part of the decommissioning and restoration process of the existing alignment, highway infrastructure will be removed. The alignment will be left with a granular surface, culverts that are no longer required will be removed as will signs and guiderail. The existing Hewitson Creek crossing will also be removed to restore an open channel.

4.1.6 Entrances

Four entrances will be affected by realigning the highway as detailed below.

A CPKC Private Entrance located approximately 570 m east of the existing park entrance south of the highway will be closed and removed. As this entrance leads towards a private CPKC railway crossing, the public is not authorized to utilize the crossing. The trail also leads toward the southern boundary of the park. Both CPKC and Ontario Parks were in agreement with the proposed closures. The entrance was signed for closure by MTO in 2021.

An entrance / trail approximately 570 m east of the existing park entrance north of the highway leading towards Rainbow Falls Provincial Park been signed for closure by MTO in 2021.

Rainbow Falls Provincial Park had no concerns with the proposed closure and no comments or concerns were received by MTO during signing.

The entrance to an MTO Aggregate Pit (Schreiber #31), approximately 1 km east of the existing park entrance will be closed.

The entrance to Hunter Road, 1.7 km east of the park entrance is under review by MTO for the potential for the closure and re-establishment of Hunter Road to the east project limits to provide a safer entrance.

4.1.7 Casque Isles Hiking Trail

During detail design, discussions were ongoing with Casque Isles Hiking Club (CIHC) to determine impacts to the existing trail within the project limits and to establish a new safe crossing within the new highway alignment. Based on consultation with CIHC and Ontario Parks, a new trail crossing along the new highway alignment was established approximately 577 m east of the new entrance to Rainbow Falls Provincial Park (Figure 12). The new trail alignment will follow a portion of the new park entrance road to the north. The existing trail will be realigned to safely cross the highway within the new MTO Right of Way. During construction, the segment of the trail within the project limits will be temporarily closed and rerouted to a temporary trail crossing.

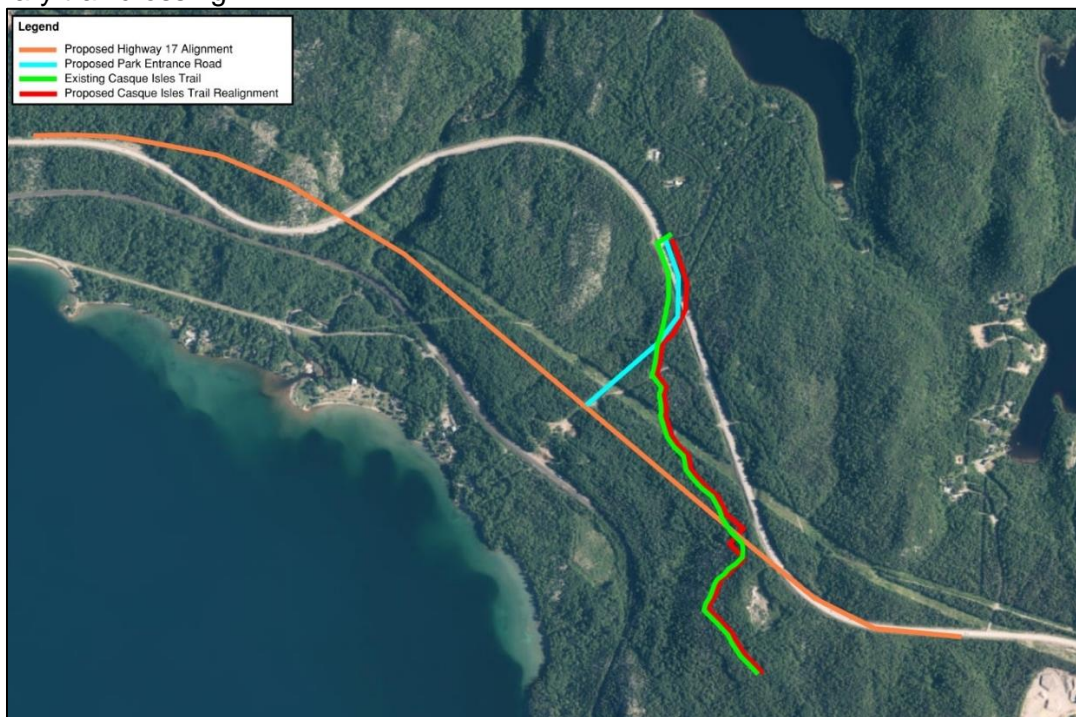


Figure 12. Casque Isles Hiking Trail Realignment

4.1.8 Vegetation Clearing

Vegetation clearing will be required throughout the project limits to construct the new highway alignment, new park access road, new parking area, for grading purposes, and for utility relocations. Some vegetation along the centerline of the new highway alignment has previously been removed to accommodate preliminary investigations to a width of approximately 10 m. Close cut clearing will be undertaken for the full width of the new right-of-way limits.

4.1.9 Roadside Safety Improvements

New guide rail will be installed along the new highway alignment as warranted for protection from embankment hazards. All guiderail along the existing highway alignment will be removed. All signage within the project limits will be upgraded as required in accordance with the Ministry's standards and policies.

4.1.10 Utilities

There will be impacts to utilities including aerial Bell lines and Hydro One distribution lines. Utilities that will be impacted by the realignment will be relocated to the new highway alignment in a staged approach. Utilities conflicting with construction will be relocated prior to construction with final utility relocations to the new highway alignment scheduled to occur post construction. A Hydro One transmission corridor parallels the new highway alignment. The new alignment does not conflict with the Hydro One transmission corridor and measures will be implemented to protect the facility during construction.

4.1.11 Construction Staging

The new highway alignment will be constructed offline while the existing alignment remains in place. Traffic will remain on the existing alignment until construction of the new alignment is complete. Staged construction will be required to tie in the west and east ends of the alignment and at locations where the existing and new alignment intersect. Staged construction will be required during the construction of the new entrance to Rainbow Falls Provincial Park. It is anticipated that the construction staging will generally maintain two lanes of traffic with short durations of single lane traffic controlled by Traffic Control Personnel. No full road closures will be required other than short duration closures to accommodate blasting.

5. Environmental Issues and Commitments

5.1 Surface / Groundwater

Existing information relating to groundwater and surface water for the project limits are discussed in the following sections.

Wells

Two drinking water wells, one domestic and one commercial, are located within 500 m of proposed blasting locations as identified through Ontario Well Records mapping. Blasting within

proximity to drinking water wells has potential to impact recharge rates and the production capacity of the well.

Groundwater

Construction activities such as dewatering during culvert works have potential to impact groundwater sources within the project limits as a result of changes to the water table. The discharge of water during dewatering operations has potential to introduce contaminants to groundwater sources.

The source of a groundwater upwelling was identified south of the existing Highway 17 alignment. The upwelling forms a channel which intersects the new highway alignment 150 m west of the new park road. The watercourse was identified by nearby property owners as a water-taking source and was confirmed to be direct fish habitat.

Water-Taking during Highway Construction

Water will be required during highway construction for asphalt operations, cooling equipment, compaction of earth and granular material for grading and culvert replacements, and for dust suppression throughout the duration of construction. These water takings are eligible for registration on the MECP Environmental Approval and Sector Registry (EASR) under O. Reg. 63/16 – Registrations Under Part II.2 of the Act – Water Taking. No mitigation is required for this activity.

5.1.1 Mitigation

Wells

During construction, the monitoring of wells will be required when blasting is occurring within proximity to drinking water wells in accordance with Ministry standards. A pre-blasting survey will be completed prior to blasting to identify any other nearby wells, utilities, and other infrastructure.

Groundwater

Throughout the project limits, the volume of groundwater dewatering at any location is not anticipated to exceed 50,000L/day. As a result, a Permit to Take Water is not presently warranted given the proposed dewatering plan. During dewatering operations, discharged water will be directed to non-erodible areas where sediment-laden water cannot re-enter watercourses. To prevent potential contamination impacts to groundwater sources during construction, standard equipment operation and maintenance measures will be implemented and are detailed further in Section 5.5.1.

5.2 Archaeology

Stage 1 and Stage 2 archaeological assessments have been completed for the project by an archaeologist licensed under the *Ontario Heritage Act* in accordance with the Ministry of Tourism, Culture, and Sport (MTCS) Standards and Guidelines for Consultant Archaeologists.

The Stage 1 assessment evaluated archaeological potential throughout the project area and the Stage 2 assessment involved a property assessment of the project area. The archaeological assessments did not identify any significant archaeological resources within the project limits and no further assessments are warranted within the subject area.

5.2.1 Mitigation

No further archaeological assessments are required. During construction, in the event archaeological resources are found, the Contractor is responsible for adhering to all requirements under the General Conditions of the contract, including suspending all operations in that area.

5.3 Built Landscapes and Cultural Heritage Features

The Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes: A Checklist for the Non-Specialist was completed for the project area during preliminary design which did not identify any built landscapes or cultural heritage resources within the project limits. As a result, no mitigation is required for built landscapes and cultural heritage features.

5.4 Memorial Cross

A memorial cross is located within the existing MTO Right of Way. The realignment of the highway will not impact the location of the memorial. Consultation with the individuals associated with the memorial occurred in detail design.

5.4.1 Mitigation

The Contractor will be advised of the location of the memorial cross and will avoid that area during construction.

5.5 Fish and Fish Habitat

Throughout the study, four watercourses were identified within the project limits as supporting fish habitat which include an unnamed watercourse, Hewitson Creek, a tributary to Hewitson Creek, and a spring-fed watercourse. A location map of the watercourses is detailed in Figure 9 (Section 4.1.3).

Upon selection of the preferred alternative and determination of the proposed footprint in fish habitat, detailed fish habitat field assessments were completed by Hatch in July 2024 to evaluate fish presence and habitat features at the four identified watercourses and update the information obtained during preliminary design. Fisheries information for the project area is summarized as follows:

Unnamed Watercourse

Unnamed watercourse is classified as a permanent, cold-water watercourse. The existing culvert is significantly perched, presenting a barrier to fish passage. Fish were not observed in the watercourse despite features suitable to support habitat for numerous species. Natural features in the channel such as boulders and step-pools likely present barriers to fish passage during low flow periods. Further downstream, the high-gradient channel restricts fish passage from Lake Superior.

Tributary to Hewitson Creek

The tributary to Hewitson Creek supports indirect fish habitat with flow originating as upland drainage and discharging into Hewitson Creek 120 m downstream of the highway culvert. At the existing highway crossing, the channel steeply slopes towards Hewitson Creek. No fish were observed during the investigations and the channel near the culvert does not provide suitable fish habitat due to the steep gradient and seasonally low water levels. Further downstream at the confluence with Hewitson Creek, the tributary may support direct fish habitat due to the direct connection to the creek and flat channel slope. This connection may provide direct fish habitat by providing nursery habitat and resting or refuge areas for salmonids in Hewitson Creek during high flow periods.

Hewitson Creek

Hewitson Creek is a permanent, cold-water watercourse with records of Brook Trout, Sculpin, dace species, and salmonid species near the existing and proposed culvert crossing. Records of Smallmouth Bass were noted in the watercourse which likely have migrated from known populations in Whitesand Lake upstream of the highway crossing. Within the system, barriers to migration exist. The downstream rail culverts restrict upstream migration from Lake Superior and the steep bedrock drops near the existing culvert crossing restricts migration further upstream. As a result, the channel supports habitat for resident Brook Trout. Fish habitat features present at the proposed new culvert crossing consist mostly of rapids and deep runs over bedrock and boulders, with some areas of slow velocities and cobble near shore which likely support rest areas and refuge for Brook Trout and forage fish.

Spring-fed Watercourse

The spring-fed watercourse is a permanent, cold-water watercourse supporting direct fish habitat. Brook Trout were present in the watercourse during the 2024 site investigations. There are barriers to migration throughout the system as downstream culverts and the natural slope of the channel restrict upstream migration from Lake Superior. Throughout the watercourse, natural barriers to fish passage are present during low flow events, however migration through the system occurs under select flow events and migration throughout the channel will be maintained upon construction of the new culvert. As a result of the migration barriers, the channel supports habitat for resident Brook Trout. Within the project area, habitat is not limiting and supports nursery and refuge functions.

There were no Aquatic Species at Risk observed during the field investigations or through the existing records. Mitigation measures for fisheries are detailed in the following sections.

5.5.1 Mitigation

To protect fish and fish habitat from impacts during construction activities near waterbodies, a summary of mitigation measures includes the following:

Design

- Fish passage is provided at all new fisheries culvert crossings and have been designed to meet MTO standards. Closed-bottom culverts will be embedded below streambed elevation.
- Open-bottom culvert at the new Hewitson Creek crossing will minimize the impacts to the watercourse.
- Low Flow channels will be installed in the culverts to maintain fish passage during periods of low flows.
- Existing non-structural culverts will be removed, and a natural channel will be constructed along the decommissioned alignment.
- The existing Hewitson Creek culvert will be removed to restore an open channel.

During Construction

- In-water work at direct fisheries watercourses will be completed within the in-water work timing window of June 16 – August 31 to minimize impacts to sensitive life processes of cold-water fish species. As a conservative measure, the June 16 – August 31 timing window applies to in-water work at the tributary to Hewitson Creek to prevent potential impacts to downstream direct fish habitat.
- Dewatering and temporary flow passage systems will be designed by the Contractor with input from qualified environmental monitors and implemented prior to any in-water work.
- In-water work will be done in isolation from the watercourses.
- Where warranted, fish salvage will be conducted in accordance with the conditions under a Licence to Collect Fish for Scientific Purposes, issued by MNR prior to conducting in-water work including dewatering.
- During dewatering operations, intake hoses will be fitted with screens to prevent incidental entrainment and impingement of fish as per Ministry standards.
- Use of explosives near fish habitat will follow the Guideline for the Use of Explosives in and Near Canadian Fisheries Waters.
- Qualified environmental monitors will ensure the mitigation strategies are implemented effectively and where necessary, will be field-fit to meet MTO standards.
- Placement of material below the high-water mark will be minimized to the extent required to complete the work.

Riparian Vegetation

- Removal of riparian vegetation will be limited to the extent required to conduct the work.
- Seed and riparian plantings to be completed within riparian zones to reestablish shade, where required.
- Clearing near sensitive fish habitat in high gradient areas will be conducted by hand where feasible, and grubbing will be deferred until grading operations are required.

Erosion and Sediment Control

- An Erosion and Sediment Control Plan will be developed by the Contractor.
- Temporary erosion control measures will apply to earth excavation within proximity to sensitive fish habitat.
- Erodible materials or exposed earth surfaces will be capped with temporary erosion control measures or isolated from watercourses with sediment controls.
- Where schedule allows, final cover will be installed immediately upon completion of earth excavation and grading to reduce duration of exposed erodible materials.
- Sediment-laden water will be pumped to a sediment containment area or vegetated area to allow water to filter prior to re-entering the watercourse.
- Discharged water will not be directed to erodible surfaces or sensitive areas.

Equipment Maintenance, Spills

- Equipment refueling and maintenance shall take place at locations as far away as practical from a waterbody and in a manner that prevents sediment and other deleterious substances from entering into a waterbody. Secondary fuel containment measures are required on all stationary equipment near watercourses.
- Equipment shall arrive on site in a clean condition.
- Where possible, equipment will be operated on dry land in a manner that minimizes disturbance to waterbody banks and riparian vegetation areas.
- Equipment used for the work in waterbodies or on waterbody banks shall be free of excess or leaking fuel, lubricants, coolant and any other deleterious substances that could enter the waterbody.
- All stockpiled materials, including but not limited to excavated overburden and topsoil, excess materials, construction debris and containers shall be stored and stabilized in a manner that prevents them from entering any waterbody.

5.6 Terrestrial Ecosystem

5.6.1 Vegetation and Natural Heritage Values

The land cover surrounding the project site consists predominantly of Bedrock-Mixed deciduous and conifer forests, with the dominant forest types within the project limits consisting of White Birch-Balsam Fir dominated mixed forests. No wetlands or significant woodlands were identified throughout the project area. Ontario Parks/ MECP has records of five rare plant species within Rainbow Falls Provincial Park (arctic-alpine disjunct species), three of which are present within the Whitesand Lake portion of the park. No significant vegetation such as arctic-alpine disjunct plant species were identified within the project limits during field investigations.

5.6.2 Wildlife

No significant wildlife habitats, rare or significant species, or features were identified at the project site. During the field investigation, the following wildlife observations were made:

- Signs of mammals including moose, deer, bear, fox, and wolf were observed throughout the study area (tracks, scats).
- No reptiles or amphibians were observed, no marsh habitats within project limits.

- Bird calls observed were associated with migratory songbirds common throughout the region.
- No nests were observed in culverts or adjacent vegetation.
- No large cavities suitable to support Pileated woodpecker nesting were observed throughout the study area.
- Forested areas provide suitable nesting habitat for numerous migratory birds.

5.6.3 Species at Risk

Woodland Caribou

During preliminary design, MECP confirmed the absence of Woodland Caribou Category 1 habitat within 10 km of the project. The General Habitat Description for Woodland Caribou was considered to evaluate the potential for the project limits to support Woodland Caribou Category 2 and Category 3 habitat. The field sampling program documented vegetation species and age classes throughout the project limits, which were not consistent with Woodland Caribou Category 2 or Category 3 habitat requirements. Forest communities within the project limits consist of immature to mature with White Birch-Balsam Fir mixed wood forests and dense understory. Woodland caribou require large (>100 km²) mature conifer forests and are not typically associated with areas with high anthropogenic disturbances or habitats that support moose, white-tailed deer, and bears.

Species at Risk Bats

To identify the potential for species at risk bats with the project limits, a review of vegetation and habitats within the project limits was undertaken during detail design. Forest stands were evaluated to identify features (age classes, level of decay, species composition) suitable to support bat roosting habitat. Low densities of snags meeting those criteria were identified and the percentage of mature forest throughout the project limits is low. The dominant vegetation species and age classes observed consisted of small to mid-sized white birch and balsam fir, few trembling aspen, abundant shrubs, which are not conducive to suitable roosting habitat.

Other Species at Risk

No species at risk or critical habitat for species at risk were observed during the field investigations. There was no evidence of species at risk birds nesting on Hewitson Creek culvert or other concrete culverts within the project limits. There is no history of bank swallow nesting in adjacent MTO Aggregate Pits, as confirmed by MTO.

5.6.4 Mitigation

To minimize potential impacts to nesting migratory birds and roosting bats during construction, vegetation clearing will be conducted outside of migratory bird nesting period and bat roosting period of April 21 – August 31.

During construction, the Contractor will avoid disturbing or destroying active nests of migratory birds.

The existing Hewitson Creek culvert provides suitable nesting habitat for migratory birds such as barn swallow. During work at the existing Hewitson Creek culvert within the migratory bird nesting period of April 21 – August 31, the culvert will be inspected daily for barn swallow nesting evidence.

5.7 Noise

During preliminary design, a Noise Impact Assessment was conducted in accordance with Ministry guidelines to identify noise impacts on adjacent sensitive receptors (residences) during construction and from the new highway realignment. The assessment concluded that the noise levels from the new alignment on the adjacent residences would remain below the levels identified in Ministry standards and additional mitigation was not warranted.

Construction noise is anticipated during operations such as crushing, pavement removal, blasting, general equipment uses. The noise assessment concluded that noise during construction would not result in significant impacts to adjacent residences and standard construction noise mitigation is warranted.

5.7.1 Mitigation

To mitigate noise during construction, standard mitigation and constraints will be adhered to, which includes proper maintenance and operation of equipment, and consideration of timing of work.

5.8 Waste and Surplus Earth Materials

Soil sampling has been completed throughout the project limits to meet the requirements under O.Reg 406/19. Earth generated during construction that is considered soil is subject to management in accordance with O.Reg 406/19. Excess earth from highway construction projects may contain elevated concentrations of salt.

During preliminary design, the TESR identified potential fugitive tailings (mining waste) adjacent to the proposed new highway alignment. The proposed project limits will avoid this area which will remain on Crown land south of the project limits and are not under the responsibility of the MTO. As such, no further mitigation is required.

5.8.1 Mitigation

Management of waste and excess material such as removed asphalt, existing guiderail, and excavated earth will be in accordance with Ministry standards and in compliance with relevant provincial legislation.

Waste

All construction debris will be removed on a regular basis. Stockpiles will be located and isolated to ensure material will not enter any watercourse. Management of all materials including disposal will be in accordance with Ministry standards and applicable regulations.

Excess Soils

Excess materials generated from the project will be reused within the project limits to the greatest extent possible to minimize haul distances and unnecessary greenhouse gas emissions. Where possible, material will be reused including specific restoration requirements along the new recreational trail within the future park boundary upon decommissioning of the highway alignment. The Contractor will manage any excess soils in accordance with provincial regulations as required.

5.9 Emergency Spill Response

Construction activities have potential to result in contamination of air, soil, groundwater, and/or surface water. While working near water, spills have potential to result in damage and contamination of fish habitat from release of sediment, fuels, and lubricants from equipment.

5.9.1 Mitigation

Direct responsibility for containment and clean-up of spills and abandoned materials on MTO highway facilities rests with the owner of the material and person in control of the material at the time of the spill or abandonment.

Where spills or abandoned materials occur on MTO highway facilities, MTO may assist where persons legally responsible cannot be located or are not able to respond. MTO assistance may include notification of authorities, provision of equipment and materials, and traffic management.

The Contractor will have an Environmental Incident Management Plan. Immediate notification of the Environmental Incident including a sediment release in fish habitat, or a spill will be made to MECP.

6. Monitoring

6.1 Environmental Protection during Construction

Qualified environmental monitoring will be carried out during specific works near watercourses such as culvert installations or removals. Monitoring activities may include inspection of erosion and sediment control measures, inspection of in-water work activities, conducting fish salvages, and confirmation of compliance with environmental mitigation measures.

7. Summary of Environmental Concerns and Commitments

The following table has been developed to summarize how outstanding issues, concerns, and previous commitments will be mitigated during construction.

Table 3. Summary of Environmental Concerns and Commitments.

I.D. #	Issues / Concerns / Potential Effects	Concerned Agencies	I.D. #	Mitigation / Protection / Monitoring
1.0	Fish and Fish Habitat			
		MTO DFO MNR Public	1.1	<ul style="list-style-type: none"> In-water timing restriction for coldwater watercourses (June 16 – August 31) applies to in-water work at waterbodies within project limits.
			1.2	<ul style="list-style-type: none"> Waterbody material placed below the high-water mark will be clean and free of debris, fines, and contaminants.
			1.3	<ul style="list-style-type: none"> Fish salvages will be conducted by a licenced individual as required during in-water work
			1.4	<ul style="list-style-type: none"> Fish screens will be installed over water intake hoses during de-watering operations in watercourses
			1.5	<ul style="list-style-type: none"> Fisheries protection measures and oversight during construction will be carried out by a qualified environmental monitor
			1.6	<ul style="list-style-type: none"> In-water work will occur in isolation of watercourses during culvert installations / removals.
			1.7	<ul style="list-style-type: none"> Where required, fish passage will be provided through temporary diversion pipes and channels.
			1.8	<ul style="list-style-type: none"> Water discharged during dewatering operations will be directed to non-erodible areas such as vegetated areas or a sediment containment system to prevent sediment-laden water from re-entering adjacent watercourses.

I.D. #	Issues / Concerns / Potential Effects	Concerned Agencies	I.D. #	Mitigation / Protection / Monitoring
			1.9	<ul style="list-style-type: none"> Erosion and Sediment Control Plans and a Dewatering and Temporary Flow Plans will be prepared by the Contractor for work near and in watercourses
			1.10	<ul style="list-style-type: none"> Temporary erosion and sediment control measures will be installed prior to work near watercourses and will be inspected and maintained to ensure proper function throughout construction.
			1.11	<ul style="list-style-type: none"> Removal of vegetation within riparian zones will be minimized as much as possible. Vegetative buffers will be maintained within riparian zones as practical to minimize potential bank erosion.
			1.12	<ul style="list-style-type: none"> Disturbed creek banks and riparian areas will be stabilized and restored with temporary erosion control measures or permanent cover to reinstate existing conditions.
			1.13	<ul style="list-style-type: none"> Rock excavation in proximity to fish habitat will be conducted as per the Guidelines for the Use of Explosives in and Near Canadian Fisheries Waters.
			1.14	<ul style="list-style-type: none"> Where possible equipment will be operated on dry land above the high-water mark and will not enter or be operated on waterbody banks to minimize disturbance to the watercourse.
			1.15	<ul style="list-style-type: none"> Constraints related to the timing and type of equipment used for clearing vegetation along waterbody banks will be applied in proximity to sensitive watercourses in high gradient areas.
			1.16	<ul style="list-style-type: none"> Debris will be isolated and contained to prevent entry into watercourses during construction.
			1.17	<ul style="list-style-type: none"> Equipment used for work near watercourses will be clean and free of contaminants such as leaking fuel, debris, coolants, or other deleterious substances.
			1.18	<ul style="list-style-type: none"> Materials such as solvents, primers, grout, degreasers, and other chemicals will be used and stored in a manner where they will not enter watercourses.

I.D. #	Issues / Concerns / Potential Effects	Concerned Agencies	I.D. #	Mitigation / Protection / Monitoring
2.0	Terrestrial Ecosystems	MTO MECP MNR	2.1	<ul style="list-style-type: none"> Existing access roads will be utilized during construction where available to minimize impacts to adjacent vegetation.
			2.2	<ul style="list-style-type: none"> Removal of vegetation will be minimized to the extent required to carry out the work.
			2.3	<ul style="list-style-type: none"> Earth slopes within the project limits will be graded to stable slopes and treated with erosion control measures or stable, non-erodible materials.
			2.4	<ul style="list-style-type: none"> Earth areas disturbed during construction to be treated with temporary erosion control measures or final cover, as feasible.
			2.5	<ul style="list-style-type: none"> Restoration plans for Park land and Crown land to be developed by MTO in consultation with MNR and MECP. Disturbed earth areas within Crown land to be restored with Northern Ontario Seed Mix. Disturbed earth areas in park boundary to be restored with Native Seed Mix and tree/shrub plantings to reflect existing conditions.
			2.6	<ul style="list-style-type: none"> Vegetation clearing will be avoiding within the bat roosting and migratory bird nesting period of April 21 – August 31.
			2.7	<ul style="list-style-type: none"> The Contractor will not destroy or disturb active bird nests.
			2.8	<ul style="list-style-type: none"> During work at the existing Hewitson Creek culvert within the migratory bird nesting period of April 21 – August 31, the culvert will be inspected daily for barn swallow nesting evidence.
3.0	Surface and Groundwater			
		MTO MECP MNR	3.1	<ul style="list-style-type: none"> Water taking for highway construction uses will be conducted in accordance with O.Reg 245/11 and O.Reg 63/16. An EASR submission will be made by the Contractor for water taking required for compaction, dust suppression, seeding.

I.D. #	Issues / Concerns / Potential Effects	Concerned Agencies	I.D. #	Mitigation / Protection / Monitoring
		Public	3.2	<ul style="list-style-type: none"> Groundwater dewatering for the purposes of culvert installations will not exceed 50,000 L per day, thereby a Permit to Take Water or EASR for Groundwater Dewatering are not required during construction.
			3.3	<ul style="list-style-type: none"> Drinking water wells within proximity to blasting locations will be identified and monitored during blasting operations.
			3.4	<ul style="list-style-type: none"> Blasting near utilities, infrastructure, wells will be subject to pre-construction surveys and protection requirements as per Ministry standards.
			3.5	<ul style="list-style-type: none"> Secondary containment on stationary equipment within 30 m of watercourses to prevent contamination of groundwater sources
			3.6	<ul style="list-style-type: none"> Equipment maintenance including spills and refuelling to be conducted as per Ministry standards to prevent impacts to waterbodies.
			3.7	<ul style="list-style-type: none"> Discharged water during dewatering operations will be directed to a sediment control measure or vegetated areas where it cannot re-enter watercourses prior to sediment filtering out.
4.0	Noise			
		Public MTO	4.1	<ul style="list-style-type: none"> Timing constraints will apply to noise from construction activities to minimize impacts to adjacent residents. Equipment idling will be restricted to the minimum level necessary to perform the work. Equipment must be maintained in good operating order to reduce excessive noise.
5.0	Archaeology / Cultural Heritage			
		MHSTCI / MCM MTO	5.1	<ul style="list-style-type: none"> During construction, in the event archaeological resources are found, the Contractor is responsible for adhering to all requirements under the General Conditions of the contract.

I.D. #	Issues / Concerns / Potential Effects	Concerned Agencies	I.D. #	Mitigation / Protection / Monitoring
6.0	Emergency Spill Response			
		MTO MECP DFO MNR	6.1	<ul style="list-style-type: none"> The Contractor will have an Environmental Incident Plan and Spill Management Plan as a requirement of General Conditions of Contract.
			6.2	<ul style="list-style-type: none"> During an environmental incident, the area will be immediately contained to prevent the material, pollutant, deleterious substance from entering adjacent environment.
			6.3	<ul style="list-style-type: none"> Spills including sediment releases will be immediately reported to MECP.
			6.4	<ul style="list-style-type: none"> In the event of a spill, cleanup and restoration of the environmental will be done to existing conditions.
7.0	Earth Management			
		MTO MECP	7.1	<ul style="list-style-type: none"> Excavated surplus earth materials will be reused during the decommissioning of the existing MTO Right-of-Way as much as possible to minimize haul distances.
			7.2	<ul style="list-style-type: none"> The Contractor will manage any earth offsite in accordance with provincial regulations including O.Reg 406/19.
8.0	Waste Management			
		MECP MTO	8.1	<ul style="list-style-type: none"> Stockpiles of excess materials including but not limited to excavated overburden and topsoil, excess materials, construction debris and containers will be located and isolated to prevent materials from entering watercourses.
			8.2	<ul style="list-style-type: none"> Excavated waste material that is suitable for re-use will be used within the project limits where feasible
9.0	Traffic Management			
		RFPP MECP	9.1	<ul style="list-style-type: none"> Construction staging plan will allow for the existing Highway 17 and park road to remain open to traffic while the realignment is constructed.

I.D. #	Issues / Concerns / Potential Effects	Concerned Agencies	I.D. #	Mitigation / Protection / Monitoring
		Public MTO		<ul style="list-style-type: none"> Temporary single-lane traffic disruptions will occur during the transition from existing highway to new highway alignment No full closures of Highway 17 are proposed during construction apart from short-duration closures during blasting.
10	Utilities			
		MTO Hydro One Bell	10.1	<ul style="list-style-type: none"> Utility conflicts with Bell and Hydro were identified. During construction, utilities will be relocated in phased approach with those impacting construction relocated prior to the construction of the highway realignment. Phase 2 will occur once the new alignment is constructed during the decommissioning of the existing highway.
12	Monitoring			
		MTO MNR	12.1	<ul style="list-style-type: none"> Qualified environmental monitoring will be carried out during specific works near watercourses for the protection of fisheries including inspection of erosion and sediment control measures, inspection of in-water work activities, conducting fish salvages, and confirmation of compliance with environmental mitigation measures.

8. Reference Documents

Hatch. 2021. GWP 6333-14-00. Transportation Environmental Study Report – Highway 17 Realignment, Hewitson Creek Culvert

Ministry of Transportation of Ontario. 2020. Environmental Guide for the Protection of Fisheries

Ministry of Transportation of Ontario. 2000. Class Environmental Assessment for Provincial Transportation Undertakings

Ministry of Transportation of Ontario. 2020. Best Management Practices for Fisheries

Ministry of Transportation. 2013. Environmental Reference for Highway Design

Appendix A

Notices and Mailing Lists

APPENDIX A1.1 Agency E-mail / Letter Mail

HATCH

H/372831

E-mail: «Email»

«Title» «First_Name» «Last_Name»

«Job_Title»

«Organization»

«Department»

«Address_1»

«Address_2»

«City», «Province» «Postal_Code»

«GreetingLine»

RE: Notice of Study
Detail Design of Hwy 17 Realignment and Hewitson Creek Culvert Replacement
Hwy 17, 9.3 km West of Schreiber, ON
MTO file: GWP 6333-14-00

The **Ontario Ministry of Transportation (MTO)** has retained **Hatch Ltd** to complete a detail design study and Class Environmental Assessment for the realignment of a 3.8 km segment of Highway 17 and the replacement of the Hewitson Creek culvert, located 9.3 km west of Schreiber, ON. The study also includes the addition of fully paved shoulders for approximately 3 km on Highway 17 west of the realignment (please refer to Location Map attached).

The purpose of this letter is to introduce the project to you and invite any comments or concerns regarding the proposed work.

PROJECT DESCRIPTION

MTO completed the planning and preliminary design study for the project in October 2021 which included the notice of completion of a Transportation Environmental Study Report (TESR). During preliminary design, the following alternative was selected to carry forward into detail design:

- Proposed realignment of Hwy 17 south of the hydro corridor.
- Proposed Hewitson Creek culvert crossing along the new alignment.
- Decommission existing highway alignment and existing Hewitson Creek culvert.
- Proposed new entrance road and parking area at Rainbow Falls Provincial Park.

Construction is tentatively scheduled for the 2025-26 construction seasons.

STUDY PROCESS

This study is following the approved planning process for a Group B Project under MTO's Class Environmental Assessment for Provincial Transportation Facilities (2000). The project team will prepare a Design and Construction Report (DCR) to document the study process and how commitments in the TESR and environmental protection measures have been addressed during

APPENDIX A1.1 Agency E-mail / Letter Mail

detail design. The DCR will be made available for a 30-day public comment period. A notice of submission of the DCR will be issued once it is available for public review.

MTO's environmental assessment will be completed in parallel with MECP's Class Environmental Assessment for Provincial Parks and Conservations Reserves (2004), which will satisfy the requirements of the Parks EA for work impacting Rainbow Falls Provincial Park. Upon completion of the study, and pending necessary approvals, regional priorities, and available funding, the project will proceed to construction.

COMMENTS

We are interested in any comments or concerns you may have. Please submit your comments to either:

Patrick Van Belleghem, P.Eng
Project Engineer – Project Delivery
Ministry of Transportation
615 James Street South
Thunder Bay, ON P7E 6P6
tel: 807-252-9757
e-mail: patrick.vanbelleghem@ontario.ca

Dale Wiersema, P.Eng
Principal Project Manager
Hatch Ltd
973 Balmoral Street, Suite 101
Thunder Bay, ON P7B 0E2
tel: 807-625-8728
e-mail: dale.wiersema@hatch.com

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, will become part of the public record.

Sincerely,

Erin McCulloch
Environmental Planner
Hatch Ltd.

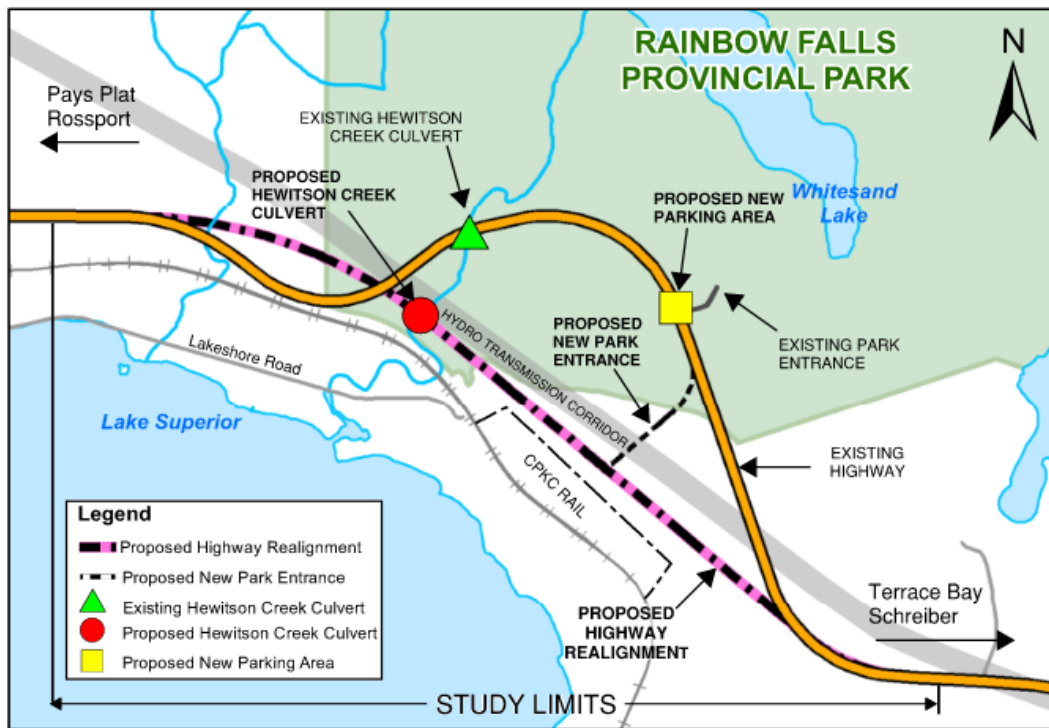
Attachment: Location Plan

cc P. Van Belleghem, MTO
T. Kleinboeck, MTO
D. Wiersema, Hatch

APPENDIX A1.1 Agency E-mail / Letter Mail

Location Plan

Detail Design of Hwy 17 Realignment and Hewitson Creek Culvert Replacement
Hwy 17, 9.3 km West of Schreiber, ON
MTO file: GWP 6333-14-00



APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
MTO Agency Contact List			
1.	Mr. Ken Coulter Senior Infrastructure Advisor Ministry of Northern Development Transportation and Infrastructure Unit 70 Foster Drive Suite 200, Roberta Bondar Place Sault Ste. Marie, ON P6A 6V8	ken.coulter@ontario.ca	
2.	Mr. David Bott Superintendent Superior North EMS North Shore Cluster District Operations	david.bott@thunderbay.ca	
3.	Fire Chief Ryan Bolan Terrace Bay Fire/Emergency Services 8 Radisson Avenue Terrace Bay, ON P0T 2W0	fire@terracebay.ca	
4.	Ms. Kimberly McNaughton District Planner Ministry of Natural Resources Nipigon District 5 Wadsworth Drive PO Box 970 Nipigon, ON P0T 2J0	kimberly.mcnaughton@ontario.ca	
5.	Ontario Provincial Police Schreiber Detachment PO Box 187 Hwy 11-17 West Nipigon, ON P0T 2J0		
6.	Mr. Anthony Friedrich Coordinator East of Thunder Bay Transportation Consortium PO Box 1300 Marathon, ON P0T 2E0	Afriedrich@sgdsb.on.ca	
7.	Mr. Jonathan Hall Chief Administrative Officer/Clerk Township of Terrace Bay 1 Selkirk Avenue & Highway 11 Terrace Bay, ON P0T 2W0	cao@terracebay.ca	

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
8.	Rosalie Evans Acting CAO/Clerk Township of Schreiber 204 Alberta Street PO Box 40 Schreiber, ON P0T 2S0	cao@schreiber.ca	
9.	Fire Chief Jan Morrison Township of Schreiber Fire and Rescue PO Box 700 222 Subway Street Schreiber, ON P0T 2S0	jmorrison@schreiberfire.ca	
10.	Traffic Staff Sergeant Cheryl Davidson NW Region Traffic Manager Ontario Provincial Police Northwest Region Box 550 15550 Hwy 17E Dryden, ON P8N 0A2	cheryl.davidson@opp.ca	<i>Please note, this notice has also been mailed to the OPP Schreiber Detachment.</i>
11.	Ministry of the Environment, Conservation and Parks Northern Region	eanotification.nregion@ontario.ca	<i>Please note that the Streamlined Project Information Form for this project is attached.</i>
12.	Ms. Karla Barboza Team Lead - Heritage Ministry of Citizenship and Multiculturalism Heritage Planning Unit 401 Bay Street, Suite 1700 Toronto, ON M7A 0A7	karla.barboza@ontario.ca	<i>Stage 1 and Stage 2 archaeological assessments were completed by Northwest Archaeological Assessments for the project following Archaeological Guidelines.</i>
13.	Joseph Harvey Heritage Planner Heritage Branch - Citizenship Inclusion and Heritage Division Ministry of Citizenship and Multiculturalism	Joseph.Harvey@ontario.ca	
14.	Jack Mallon Heritage Planner Ministry of Citizenship and Multiculturalism Heritage Planning Unit 401 Bay Street, Suite 1700 Toronto, ON M7A 0A7	Jack.mallon@ontario.ca	<i>Stage 1 and Stage 2 archaeological assessments were completed by Northwest Archaeological Assessments for the project following Archaeological Guidelines.</i>
15.	Ms. Katherine Kirzati Heritage Planner Ministry of Citizenship and Multiculturalism Heritage Planning Unit 401 Bay Street, Suite 1700 Toronto, ON M7A 0A7	katherine.kirzati@ontario.ca	<i>Stage 1 and Stage 2 archaeological assessments were completed by Northwest Archaeological Assessments for the project following Archaeological Guidelines.</i>

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
16.	Matt Borutski President Casque Isles Hiking Trail Club		
17.	Voyageur Trail Association PO Box 22009 RPO Cambrian Sault Ste. Marie, ON P6B 0E9	info@voyageurtrail.ca	<i>Please note that this Notice has also been sent to the Casque Isles Hiking Trail Club.</i>
18.	Ms. Dawn King Secretary/Treasurer Rossport Local Services Board PO Box 62 Rossport, ON P0T 2R0	rossportlsb@gmail.com	
19.	Mr. Mathieu Roy Vice President, Trail Development and Managmegnt Trans Canada Trail 300-321 de la Commune W Montreal, QC H2Y 2E1		<i>Please note that this Notice has also been sent to the Casque Isles Hiking Trail Club and the Voyageur Trail Association.</i>
20.	Ms. Katherine Onyshkewych Senior Park Planner Ministry of the Environment, Conservation and Parks Ontario Parks 435 James St. S Suite 221D Thunder Bay, ON P7E 6S7	katherine.onyshkewych@ontario.ca	
21.	Mr. Brian Phelan Provincial Lands Specialist Ministry of the Environment, Conservation and Parks Ontario Parks 435 James St. S Suite 221D Thunder Bay, ON P7E 6S7	brian.phelan@ontario.ca	
22.	Mr. Neil Hillis Assistant Zone Ecologist (Acting) Ministry of the Environment, Conservation and Parks Ontario Parks 435 James St. S Suite 221D Thunder Bay, ON P7E 6S7	neil.hillis@ontario.ca	

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
23.	Ms. Susan Golets Director (Acting) Heritage, Sport, Tourism and Culture Industries Policy Branch College Park 18th Flr 777 Bay Street Toronto, ON M7A 1S5	susan.golets@ontario.ca	
24.	Filanes Fallen Rock Motel PO Box 460 Schreiber, ON P0T 2S0		
25.	Mr. Marc Stewart Senior Manager Ministry of Mines Rehabilitation Section Willet Green Miller Ctr Level B6 Unit 933 Ramsey Lake Rd Sudbury, ON P3E 6B5	marc.stewart@ontario.ca	
26.	Allison Spadoni Superintendent Ministry of Environment, Conesrvation, and Parks Ontario Parks Operations PO Box 280 Terrace Bay, ON P0T 2W0	allison.spadoni@ontario.ca	
27.	Emilie Trottier Mine Hazards Technical Specialist Ministry of Mines Mine Rehabilitation Group - Mineral Development Branch	emilie.trottier@ontario.ca	
28.	Nafey Gill CPKC Public Works Maneger CPKC	Nafey.gill@cpkcr.com	
MECP – Ontario Parks Contact List			
29.	Animal Alliance of Canada 101 - 221 Broadview Avenue Toronto, Ontario M4M 2G3	contact@animalalliance.ca	
30.	Office of the Auditor General of Ontario 20 Dundas Street West, Suite 1530 Toronto, Ontario M5G 2C2	chris.wilkinson@auditor.on.ca	

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
31.	Tracy Tucker Executive Assistant/Office Manager Canadian Environmental Law Association 55 University Ave., Suite 1500 Toronto, Ontario M5J 2H	tracy@cela.ca	
32.	Canadian Herpetological Society C/O Jolene Lavery Nova Scotia Department of Lands and Forestry	info@canadianherpetology.ca	
33.	Canadian Parks & Recreation Association 1180 Walkley Road, P.O. Box 83069 P.O. Box 83069 Ottawa, Ontario K1V 2M5	info@cpra.ca	
34.	Canadian Sportfishing Industry Association 171 Rink St., Suite 102 Peterborough, Ontario K9J 2J6		
35.	Ducks Unlimited 740 Huronia Road, Unit 1 Barrie, Ontario L4N 6C6	du_barrie@ducks.ca	
36.	Earthroots 401 Richmond Street West, Suite 410 Toronto, Ontario M5V 3A8	info@earthroots.org	
37.	Ecojustice PO BOX 106 1910 - 777 Bay St Toronto, Ontario M5G 2C8	communications@ecojustice.ca	
38.	Federation of Ontario Cottagers' Associations (FOCA) 201 - 159 King Street Peterborough, Ontario K9J 2R8	info@foca.on.ca	
39.	Forests Ontario 15 Maple Ave Unit #103 Barrie, Ontario L4N 2N6	info@forestsontario.ca	
40.	Bill Mungall Hike Ontario 262 Lavender Drive Ancaster, Ontario L9K 1E5	info@hikeontario.com	
41.	Hydro One Inc. - Environmental Services & Approvals	regulatory@hydroone.com	

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
	8th Floor Reception, 483 Bay St South Tower Toronto, Ontario M5G 2P5		
42.	International Mountain Bicycling Association Canada (IMBA) PO BOX 1131 Garibaldi Highlands, British Columbia V0N 1T0	info@imbacanada.com	
43.	Colin Hovi Team Lead Mineral Exploration and Development Section Ministry of Mines Thunder Bay Office B002, 435 James St S Thunder Bay, Ontario P7E 6S7	colin.hovi@ontario.ca	
44.	Glen Niznowski Manager Northern Region Office Ministry of Environment, Conservation and Parks MECP - Northern Region 3rd Floor 331B - 435 James St S Thunder Bay, Ontario P7E 6S7	glen.niznowski@ontario.ca	
45.	Bryce Voca Supervisor Thunder Bay District Office Ministry of Environment, Conservation and Parks 3rd Flr Suite 331B, 435 James St S Thunder Bay, Ontario P7E 6S7	bryce.voca@ontario.ca	
46.	Kim Greonendyk Director (Acting) Negotiations and Reconciliation Division Negotiations Branch - Northwest Ministry of Indigenous Affairs 9th Flr, 160 Bloor St E Toronto, Ontario M7A 2E6	kim.greonendyk@ontario.ca	
47.	Nature and Outdoor Tourism Ontario (NOTO) 386 Algonquin Ave North Bay, Ontario P1B 4W3	info@noto.net	
48.	Ontario Chapter- Sierra Club of Canada Foundation	ontario@sierraclub.ca	

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
	PO BOX 2007 Station B Ottawa, Ontario K1P 5W3		
49.	Ontario Federation of All Terrain Vehicle Clubs (OFATV) PO Box 299 Coe Hill, Ontario K0L 1P0	info@ofatv.org	
50.	Ontario Federation of Anglers and Hunters PO Box 2800 4601 Guthrie Dr Peterborough, Ontario K9J 8L5	ofah@ofah.org	
51.	Murray Baker Ontario Federation of Snowmobile Clubs 501 Welham Road, Unit 9 Barrie, Ontario L4N 8Z6	inquiries@ofsc.on.ca	
52.	Rob Girouard Ontario Federation of Trail Riders 3000 Steeles Ave East, Suite 201 Markham, Ontario L3R 4T9	info@oftr.ca	
53.	Ontario Forest Industries Association (OFIA) 8 King Street East, Suite 1704 Toronto, Ontario M5C 1B5	info@ofia.com	
54.	Ontario Fur Managers Federation 531 Second Line East Sault Ste Marie, Ontario P6B 4K2	furmanagers@gmail.com	
55.	Ontario Live Bait Angling Association 3005 Wasdell Falls Road Washago, Ontario L0K 2B0		
56.	Adrianna Stech Ontario Mining Association 1201 - 5775 Yonge St Toronto, Ontario M2M 4J1	info@oma.on.ca	
57.	Ontario Nature 720 Bathurst Street Toronto, Ontario M5S 2R4	info@ontarionature.org	
58.	Paul Ronan Executive Director Ontario Parks Association (OPA) 60 Water Street North Cambridge, Ontario N1R 3B1	paul@ontarioparksassociation.ca	

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
59.	Ontario Private Campground Association 306 - 230 Westney Road South Ajax, Ontario L1S 7J5	info@campinginontario.ca	
60.	Chris Baskys Ontario Recreation Mountain Bicycling Alliance - Ontario Cycling Association 2015 Pan Am Blvd. Suite #2 Milton, Ontario L9T 8Y9	support@ontariocycling.org	
61.	Ontario Recreational Canoeing and Kayaking Association (ORCKA) PO Box 5024 Huntsville, Ontario P1H 2K5	info@orcka.ca	
62.	Patrick Connor Executive Director Ontario Trails Council 500 Main Street Deseronto, Ontario K0K 1X0	execdir@ontariotrails.ca	
63.	Michelle McShane Executive Director Paddle Canada PO Box 126 Station Main Kingston, Ontario K7L 4V6	info@paddlecanada.com	
64.	Peaceful Parks Coalition 1-133 John Street London, Ontario N6A 1N7	ppc@peacefulparks.org	
65.	Prospectors & Developers Association of Canada 800-170 University Avenue Toronto, Ontario M5H 3B3	info@pdac.ca	
66.	The Conservation Council of Ontario C/O Cariporter Inc PO BOX 73021, 465 Yonge St Toronto, Ontario M4Y 2W5	ontarioconservation@gmail.com	
67.	The Nature Conservancy Of Canada - Ontario PO Box 443 London, Ontario N6A 4W1	ontario@natureconservancy.ca	
68.	Tourism Industry Association of Ontario (TIAO) 405 - 207 Queen's Quay West Toronto, Ontario	info@tiaontario.ca	

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
69.	M5J 1A7 Wildlands League 371 - 401 Richmond Street West Toronto, Ontario M5V 3A8	info@wildlandsleague.org	
70.	Cameron Mack Executive Director Wildlife Habitat Canada 247 - 2039 Robertson Rd Ottawa, Ontario K2H 8R2	admin@whc.org	
71.	World Wildlife Fund of Canada 410 Adelaide St. West, Suite 400 Toronto, Ontario M5V 1S8	ca-panda@wwfcanada.org	
Rainbow Falls Provincial Park Agency-Business Mailing List			
72.	Supervisor of Track Maintenance Canadian Pacific Kansas City Railway c/o 320 Winnipeg Ave. N. White River, ON P0M 3G0		<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
73.	Manager - Road Operations Canadian Pacific Kansas City Railway PO Box 219 Schreiber, ON P0T 2S0		<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
74.	Township of Terrace Bay Community Development Supervisor Terrace Bay, ON P0T 2W0	cds@terracebay.ca	<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
75.	Great Lakes Sea Kayaking Association 23 Van Dusen Blvd. Toronto, Ontario M8Z 3E4		<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
76.	Mikus Fuels Box 190 Terrace Bay, ON P0T 2W0		<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
77.	Chris Magee District Manager Nipigon Work Center Ministry of Natural Resources Nipigon, ON P0T 2J0	chris.magee@ontario.ca	<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
78.	Northern Computers 9 Simcoe Plaza		<i>You have been notified of this project due to past</i>

APPENDIX A1.2 Agency Mailing List

#	Contact	Email	Additional / note
	Box 1163 Terrace Bay, ON P0T 2W0		<i>interest in the Rainbow Falls Provincial Park Management Plan.</i>
79.	Finance & Special Projects Administrator Schreiber Economic Development Office Township of Schrieber PO Box 40 Schreiber, ON P0T 2S0	fspa@schreiber.ca	<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
80.	Schreiber Public Library P.O. Box 39 Schreiber, ON P0T 2S0	library@schreiber.ca	<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
81.	Superior Sno Goers Snowmobile Club Schreiber, ON P0T 2S0		<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>
82.	Terrace Bay Tourist Information Centre P.O. Box 40 1008 Highway 17 Terrace Bay, Ontario P0T 2W0	tourism@terracebay.ca	<i>You have been notified of this project due to past interest in the Rainbow Falls Provincial Park Management Plan.</i>

APPENDIX A2.1 Indigenous Groups letter

Ministry of Transportation
Director's Office
North Operations
447 McKeown Avenue
North Bay, ON P1B 9S9
705 497-5500

Ministère des Transports
Bureau du directeur
Opération - Nord
447, avenue McKeown
North Bay, ON P1B 9S9
705 497-5500



Date

Position (e.g., Chief [First Name] [Last Name])
Community Name
Address
City, Province
Postal Code

Sent via email: [recipient email address]

Subject: Notice of Study

Detail Design of Hwy 17 Realignment and Hewitson Creek Culvert Replacement
Hwy 17, 9.3 km West of Schreiber, ON
G.W.P. 6333-14-00

Dear [Title and Name (e.g., Chief [Last Name])],

The Ontario Ministry of Transportation of Ontario (MTO) has retained the services of Hatch Ltd. to undertake a Detail Design and Class Environmental Assessment for the realignment of a 3.8 km segment of Highway 17, the replacement of the Hewitson Creek culvert located 9.3 km west of Schreiber, and installation of fully paved shoulders for approximately 3 km on Highway 17 west of the realignment. A map showing the location of the study area is attached. Details include the following:

- Proposed realignment of Highway 17 bound between the Hydro corridor and CPKC Rail tracks.
- Proposed Hewitson Creek culvert crossing along new alignment.
- Decommission existing highway alignment and existing Hewitson Creek culvert.
- Proposed new entrance road and parking area at Rainbow Falls Provincial Park.

This study is being carried out in accordance with the approved environmental planning process for Group 'B' projects under the MTO *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000).

The Planning and Preliminary Design study for the project was completed in 2021, which included the Notice of Completion of a Transportation Environmental Study Report (TESR), issued in October 2021 for a public comment period. During Preliminary Design, various

APPENDIX A2.1 Indigenous Groups letter

alternatives were evaluated and an alternative consisting of the realignment of Highway 17 and a new Hewitson Creek culvert crossing was selected to carry forward into Detail Design.

The purpose of this study is to further develop the proposed alternative and commitments made in the TESR. The study includes undertaking detailed environmental and engineering field investigations, seeking input from stakeholders, external agencies, Indigenous communities and the public, and finalizing the design. A Design and Construction Report (DCR) will be prepared to document the Detail Design study process. This includes a review of the 2021 TESR to document how commitments identified in the TESR, and environmental protection measures, have been addressed during Detail Design. The DCR will be made available for a 30-day public comment period. A Notice of Submission of the DCR will be issued at that time to indicate where the DCR will be made available for review.

The purpose of this letter is to provide you with the opportunity to meet with the project team to learn further details on the project and provide input into the process. If you would like to arrange a meeting, have questions and comments, or require additional information, please contact Heather Boyer, Regional Services and Relationships Manager by email at heather.boyer@ontario.ca or by phone at (807) 630-8996. MTO will continue to engage with [community name] as this project progresses.

Information collected during the Detail Design and Class Environmental Assessment will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information and other protected information, including information outlined in section 15.1 (information received in confidence from an Indigenous community), may be included in study documentation and become part of the public record. Information received in confidence from an Indigenous community may be protected and not made public, such as information on sites of cultural significance, harvesting areas and resources, traditional ecological knowledge, and land use values. Please contact me if you have accessibility requirements in order to participate in this project.

Sincerely,

[Insert Signature]

Herb Villneff
Director, North Operations

Attachment:

- Attachment A: Location Map

c.c. H. Boyer, Regional Services and Relationships Manager, MTO

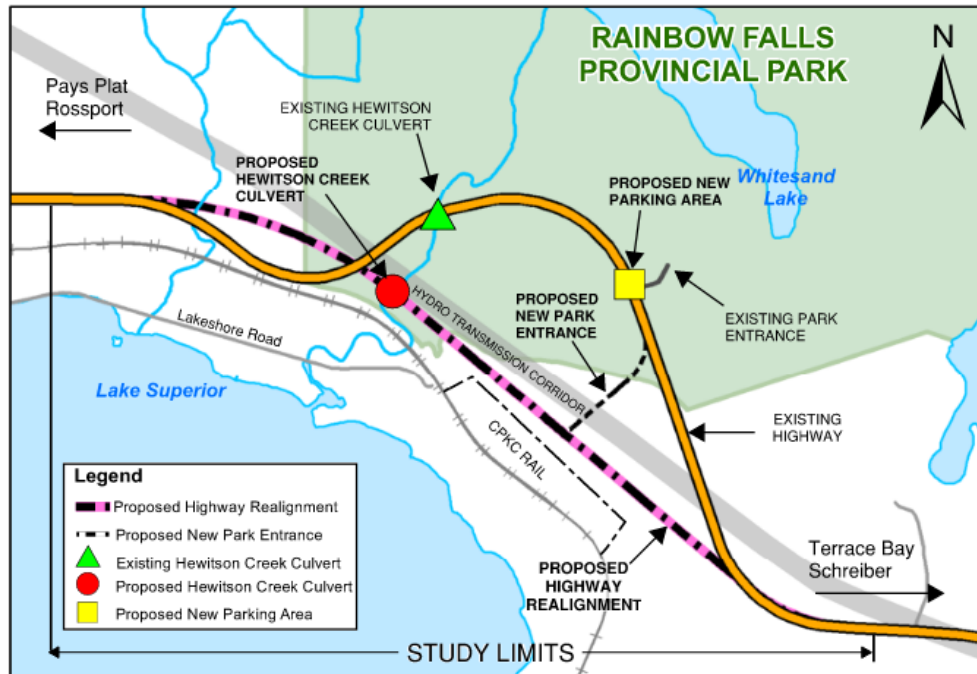
APPENDIX A2.1 Indigenous Groups letter

P. Van Belleghem, Project Engineer, MTO
M. Sanders, Indigenous Liaison Specialist, MTO

APPENDIX A2.1 Indigenous Groups letter

Attachment A: Location Map

Detail Design of Hwy 17 Realignment and Hewitson Creek Culvert Replacement
Hwy 17, 9.3 km West of Schreiber, ON
MTO file: GWP 6333-14-00



APPENDIX A2.2 Indigenous Groups Mailing List

#	Contact
Indigenous Contact List	
1.	Chief David Mushquash Pays Plat First Nation 10 Central Place Pays Plat ON P0T 3C0
2.	Chief Duncan Michano Biigtigong Nishnaabeg 78 Pic River Rd., P.O. Box 193 Heron Bay, Ontario P0T 1R0
3.	Chief Marcus Hardy Red Rock Indian Band PO Box 1030 Nipigon, Ontario P0T 2J0
4.	Chief Gladys Thompson Biinjitiwaabik Zaaging Anishinaabek 501 Spirit Bay Road General Delivery Macdiarmid, ON P0T 2B0
5.	Chief Sheri Taylor Ginoogaming First Nation PO Box 89 Longlac, ON, P0T 2A0
6.	Chief Judy Desmoulin Long Lake #58 First Nation 209 Otter Street, PO Box 609 Longlac, ON, P0T 2A0
7.	Chief Troy DeLaRonde Red Sky Métis Independent Nation 406 East Victoria Avenue Thunder Bay, ON P7C 1A5
8.	Regional Councillor Tim Sinclair, Region 2 Métis Nation of Ontario
9.	Mr. Jon MacDonald President Jackfish Métis Association Box 146 Schreiber, ON P0T 2S0

Thursday, March 28, 2024

«Field2»

Notice of Study

Detail Design of Hwy 17 Realignment and Hewitson Creek Culvert Replacement
Hwy 17, 9.3 km West of Schreiber, ON
MTO file: GWP 6333-14-00

Dear «Greeting»,

The **Ontario Ministry of Transportation (MTO)** has retained **Hatch Ltd** to complete a detail design study and Class Environmental Assessment for the realignment of a 3.8 km segment of Highway 17 and the replacement of the Hewitson Creek culvert, located 9.3 km west of Schreiber, ON. The study also includes the addition of fully paved shoulders for approximately 3 km on Highway 17 west of the realignment (please refer to Location Map attached).

The purpose of this letter is to introduce the project to you and invite any comments or concerns regarding the proposed work.

PROJECT DESCRIPTION

MTO completed the planning and preliminary design study for the project in October 2021 which included the notice of completion of a Transportation Environmental Study Report (TESR). During preliminary design, the following alternative was selected to carry forward into detail design:

- Proposed realignment of Hwy 17 south of the hydro corridor.
- Proposed Hewitson Creek culvert crossing along the new alignment.
- Decommission existing highway alignment and existing Hewitson Creek culvert.
- Proposed new entrance road and parking area at Rainbow Falls Provincial Park.

Construction is tentatively scheduled for the 2025-26 construction seasons.

STUDY PROCESS

This study is following the approved planning process for a Group B Project under MTO's Class Environmental Assessment for Provincial Transportation Facilities (2000). The project team will prepare a Design and Construction Report (DCR) to document the study process and how commitments in the TESR and environmental protection measures have been addressed during detail design. The DCR will be made available for a 30-day public comment period. A notice of submission of the DCR will be issued once it is available for public review. MTO's environmental assessment will be completed in parallel with

APPENDIX A3 Property Owner Letter

MECP's Class Environmental Assessment for Provincial Parks and Conservations Reserves (2004), which will satisfy the requirements of the Parks EA for work impacting Rainbow Falls Provincial Park. Upon completion of the study, and pending necessary approvals, regional priorities, and available funding, the project will proceed to construction.

COMMENTS

We are interested in any comments or concerns you may have. Please submit your comments to either:

Patrick Van Belleghem, P.Eng
Project Engineer – Project Delivery
Ministry of Transportation
615 James Street South
Thunder Bay, ON P7E 6P6
tel: 807-252-9757
e-mail: patrick.vanbelleghem@ontario.ca

Dale Wiersema, P.Eng
Principal Project Manager
Hatch Ltd
973 Balmoral Street, Suite 101
Thunder Bay, ON P7B 0E2
tel: 807-625-8728
e-mail: dale.wiersema@hatch.com

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, will become part of the public record.

Sincerely,



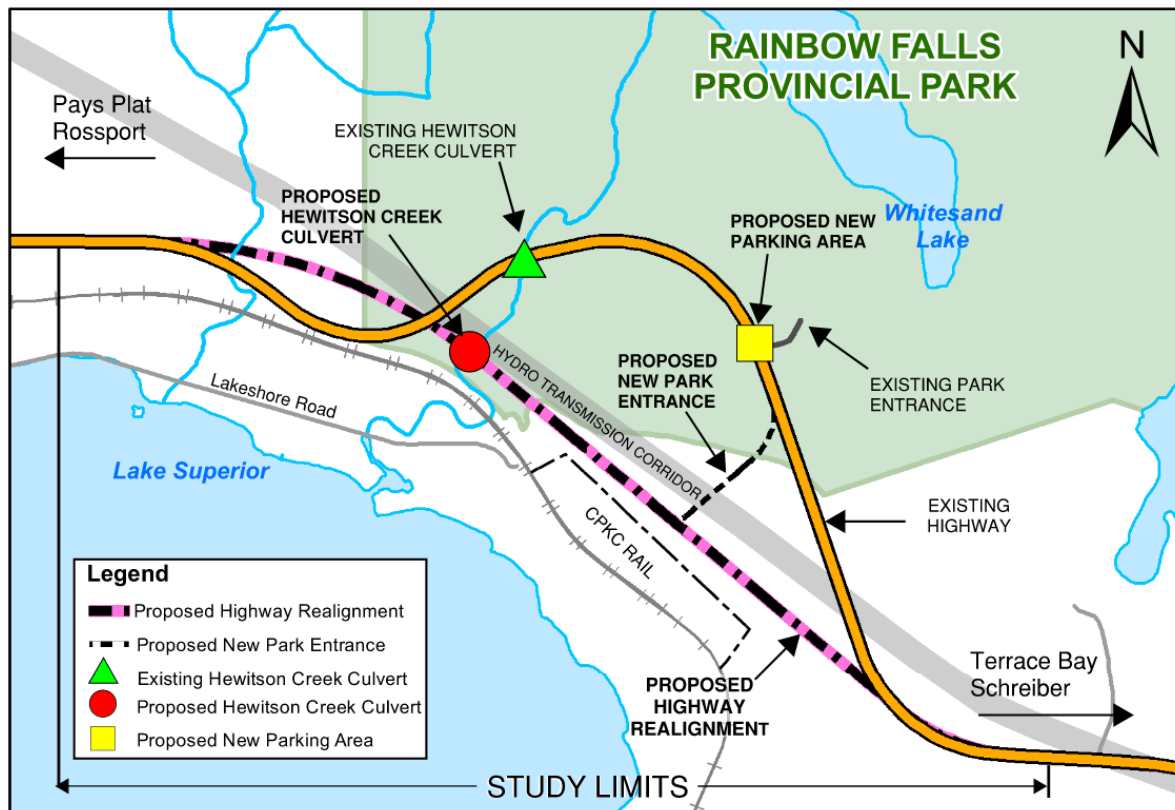
Erin McCulloch
Environmental Planner
Hatch Infrastructure

Attachment: Location Plan

APPENDIX A3 Property Owner Letter

Location Plan

Detail Design of Hwy 17 Realignment and Hewitson Creek Culvert Replacement
Hwy 17, 9.3 km West of Schreiber, ON
MTO file: GWP 6333-14-00



APPENDIX A4. Newspaper Ad

Commencement of Study Notice

Highway 17 Curve Realignment and Hewitson Creek Culvert Replacement, 9.3 km west of Schreiber

The **Ontario Ministry of Transportation (MTO)** has retained **Hatch Ltd** to complete a detail design study and environmental assessment for the realignment of a 3.8 km segment of Highway 17 and the replacement of the Hewitson Creek culvert. The purpose of this notice is to advise you about this study and request that you share any concerns or comments.

PROJECT DESCRIPTION

MTO completed the planning and preliminary design study for the project in October 2021 which included the notice of completion of a transportation environmental study report (TESR). During preliminary design, the following alternative was selected to carry forward into detail design:

- Proposed realignment of Hwy 17 south of the hydro corridor.
- Proposed Hewitson Creek culvert crossing along the new alignment.
- Decommission existing highway alignment and existing Hewitson Creek culvert.
- Proposed new entrance road and parking area at Rainbow Falls Provincial Park.

STUDY PROCESS

This study is following the approved planning process for a Group B Project under MTO's Class Environmental Assessment for Provincial Transportation Facilities (2000). The project team will prepare a design and construction report (DCR) to document the study process and how commitments in the TESR and environmental protection measures have been addressed during detail design. The DCR will be made available for a 30-day public comment period. A notice of submission of the DCR will be issued once it is available for public review. MTO's environmental assessment will be completed in parallel with MECP's Class Environmental Assessment for Provincial Parks and Conservations Reserves (2004), which will satisfy the requirements of the Parks EA for work impacting Rainbow Falls Provincial Park.

COMMENTS

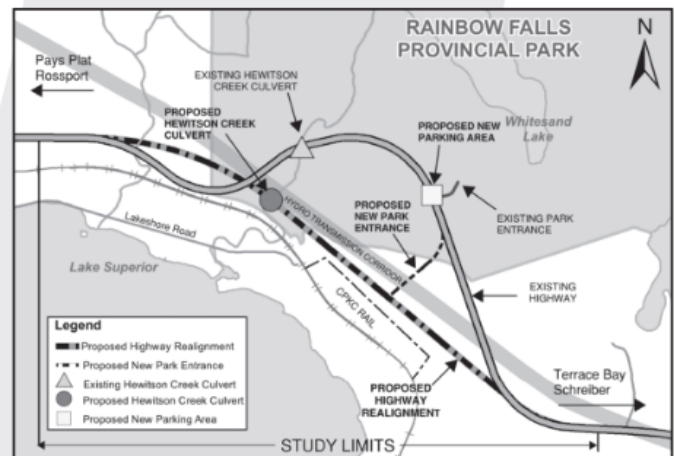
If you have any questions or concerns about the project, or would like to be added to the contact list for project notices, please contact one of the following project team members:

Patrick Van Belleghem, P.Eng
Project Engineer - Project Delivery
Ministry of Transportation
615 James Street South
Thunder Bay, ON P7E 6P6
tel: 807-252-9757
tel toll free: 1-800-465-5034
fax: (807) 473-2168
e-mail: patrick.vanbelleghem@ontario.ca

Dale Wiersema, P.Eng
Principal Project Manager
Hatch Limited
973 Balmoral Street, Unit 101
Thunder Bay, ON P7B 0E2
tel: (807) 625-8728
fax: (807) 623-5925
e-mail: dale.wiersema@hatch.com

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, will become part of the public record. If you have any accessibility requirements in order to participate in this project, please contact one of the project team members listed above.

Des renseignements sont disponibles en français en composant 1-905-486-0365, Antonio Ibarra.



Appendix B

Correspondence

APPENDIX B.1 Detail Design Correspondence

#	Contact	Comment	Response
1.	Ministry of Citizenship and Multiculturalism Ontario Public Service	<p>Comment received Apr 29, 2024:</p> <p>Dale Wiersema,</p> <p>Please find attached our initial advice on the above referenced undertaking.</p> <p>Please do not hesitate to contact me with any questions or concerns.</p> <p>A summary of letter: recommendations to complete built heritage and cultural landscape screening</p> <p>Regards,</p> <p>Joseph Harvey Heritage Planner Heritage Branch Citizenship Inclusion and Heritage Division Ministry of Citizenship and Multiculturalism Ontario Public Service</p>	<p>Response provided May 14, 2024:</p> <p>Thank you for your comments on the study.</p> <p>In response to your question, screening checklists for built heritage and cultural landscapes have been completed by a qualified person for this project, and the screening did not identify any cultural heritage features requiring further technical evaluation or assessments. The outcome of the screening will be documented in the EA report prepared for this project.</p> <p>If you have any further comments, please let us know.</p>
2.	NW Region Traffic Manager Ontario Provincial Police	Project acknowledgement.	<i>No response required.</i>
3.	District Planner Nipigon District Ministry of Natural Resources and Forestry	<p>Comment received Apr 3, 2024:</p> <p>Requested confirmation of all the previous correspondence shared from MNRF on this file</p> <p>Inquired about the status of the Park Mgmt Plan amendment, and MNRF role in this process.</p>	<p>Response provided April 17, 2024:</p> <p>Provided a zip file containing all previous MNRF correspondence on file for this project and also documented the correspondence in an attached table.</p>

APPENDIX B.1 Detail Design Correspondence

		<p>Comment received May 24, 2024:</p> <p>Requested confirmation of current MECP Parks contact on this file to facilitate MNR's understanding of Ontario Parks processes (Class EA PPCR and Park Management Plan Amendment) and confirm whether MNR is triggered in any of the pieces.</p> <p>MNR confirms that we would appreciate receiving and reviewing the Design and Construction Report when available for this project.</p>	<p>Response provided July 24, 2024:</p> <p>Provided existing Parks contacts to MNR.</p>
4.	Ministry of the Environment, Conservation and Parks Northern Region	<p>Comment received Apr 15, 2024:</p> <p>Dear Project Team,</p> <p>Thank you for providing the Ministry of the Environment, Conservation and Parks with the Notice of Commencement for the Detail Design for Hwy 17 Curve Realignment, Hewitson Creek Culvert Replacement in Killraine. Attached are proponent's guide to preliminary screening of species at risk and the acknowledgement for the proposed project. Please ensure continuous communication as required throughout the progress of the project.</p> <p>Thank you, Maya Jacewicz Assistant Project Officer Environmental Assessment and Permissions Division Environmental Assessment Branch MECP</p>	<p><i>No response required: communication with MECP will be undertaken throughout the project as required.</i></p>
5.	Nature and Outdoor Tourism Ontario (NOTO)	<p>Comment received Mar 28, 2024:</p> <p>Can we share this in our next newsletter that way our members in the area also have the opportunity to comment?</p>	<p>Response sent Apr 2, 2024:</p> <p>MTO has no problems with sharing the information from the study notice in the newsletter.</p>

APPENDIX B.1 Detail Design Correspondence

6.	Casque Isles Hiking Club	<p>Hello Dale and Patrick,</p> <p>The Casque Isles Hiking Club is very pleased with the communication and cooperation that we have received on this project, especially the advance notice of preliminary clearing of the corridor line and the recent drilling program. This has allowed us to give our users advance notice of trail closures, keeping hikers out of the work area. We look forward to conducting a site assessment when the time is right.</p> <p>We have a plan in place to close the portion of trail that crosses the construction zone and direct hikers safely around it. I am sure that we will receive sufficient notice to close the affected portion and take down any signage.</p> <p>I have walked in the area and have some idea of where we can cross the highway after the construction is complete, but I want to take [names] around there so we can come to a consensus. Because there will be a large in-fill where the trail currently crosses the realignment, I think that we should wait until later in the process to make any decision where our trail may cross post-construction. As the plan calls for wide paved shoulders for cyclists, I will ask if those wide shoulders will also be incorporated into the new access to Rainbow Falls? That would be an added safety benefit to cyclists on the new access, and potential hikers, depending on where our trail meets the new access as well.</p> <p>Thank you for keeping us in the loop.</p>	<p><i>Consultation and ongoing discussions occurred with CIHC throughout the detail design study to identify specific impacts to the trail and to develop the integration of the trail along the new park entrance road and a new safe trail crossing along the proposed Highway 17 alignment.</i></p>
7.	Property Owner	<p>Comment received April 29, 2024:</p> <p>Thank you for the opportunity to comment on the proposed highway realignment.</p> <p>When entering onto the highway from Hunter Road looking west we are aware of a low spot on the highway directly opposite to the east study limit marker on your map.</p> <p>Quite often when entering onto the highway from our entrance heading east we will look west for oncoming traffic and see none and then part way through our entry on the highway a vehicle will</p>	<p><i>Resolution: Sight distances from Hunter Road were reviewed and found to be substandard. MTO will conduct all necessary consultation, finalize design and road closure requirements to potentially re-establish access for Hunter Road at the Winston / Whitesand Lake Road. If feasible, this will be included into the contract; the Contractor will provide residents with advanced notice prior to any changes to access occurring.</i></p>

APPENDIX B.1 Detail Design Correspondence

		<p>appear out of nowhere as it was in that low area when we look initially.</p> <p>Our question is might that low area be part of the proposed realignment?</p> <p>I have witnessed many accidents along the section of highway being realigned so we are all very happy to see this work commencing next year.</p>	
--	--	---	--

APPENDIX B.2 Correspondence in response to the TESR Public Comment Period (2021/2022)

#	Contact	Post-TESR Comment	Response / Resolution
Indigenous Communities and Organizations			
1.	Jackfish Metis Association	Comment received October 2021: <ul style="list-style-type: none"> Supportive of proposed work and safety improvements, preference to include rest area along realignment. 	Response provided January 2022: <ul style="list-style-type: none"> Preferred new highway configuration similar to existing and will include truck climbing lane. Existing rest area will be within abandoned alignment and a new rest area not warranted along realigned segment.
2.	Red Sky Metis Independent Nation	Comments received December 2021 and January 2022: <ul style="list-style-type: none"> Inquired about work opportunities during construction. Noted no concerns with project regarding Treaty Rights, requested to be notified if archaeological discoveries found during construction. 	Response provided December 2021: <ul style="list-style-type: none"> Indigenous participation in procurement to be determined during detail design. MTO to provide RSMIN with project updates as design progresses.
Agencies			
3.	Filanes Motel	Comment received October 2021: <ul style="list-style-type: none"> Questioned location of alignment concern with hotel being along abandoned highway 	Response provided October 2021: <ul style="list-style-type: none"> Hotel not within section subject to realignment, no impacts.
4.	Casque Isles Hiking Club	Comment received November 2021: <ul style="list-style-type: none"> Noted preference for track of hiking trail be shown on plan Realignment of trail required to accommodate new highway alignment. Requested to be notified once construction commences to discuss trail impacts. Trail closure signs required if segment to be closed during construction. Provided suggestions for new trail alignment. Requested continuous communication on impacts to trail, trail signage. 	Response provided January 2022: <ul style="list-style-type: none"> MTO to carry out ongoing discussion and work with CIHC in detail design to determine plans and mitigation for new safe trail crossing if proposed realignment is developed.

APPENDIX B.2 Correspondence in response to the TESR Public Comment Period (2021/2022)

5.	Ministry of Mines	Comments received November 2021: <ul style="list-style-type: none"> Requested GIS files for project area. Identified potential fugitive tailings area immediately south of proposed ROW within limits of property request. 	Response provided November 2021: <ul style="list-style-type: none"> Provided GIS files of project area Noted additional investigations will be carried out to confirm presence of fugitive tailings within the proposed MTO Right of Way including preparation of a plan for removal and disposal if required.
6.	MNR	Comment received November 2021: <ul style="list-style-type: none"> Provided fisheries concerns within project limits. Concern with ownership and maintenance of new park road through Crown lands. Reference to Class EA RSFD not identified in TESR and MNR would not proceed with disposition of Crown land unless provided with proof the project met requirements under EA Act. 	Response provided January 2022: <ul style="list-style-type: none"> MTO to work with MNR and MECP on ownership of proposed park access road. Targeted fisheries studies to be undertaken in detail design, water crossings to be designed to meet MTO standards for fish passage requirements. Eligible for Environmental Clearance letter to be completed at end of study identifying how requirements of MTO Class EA process meet requirements under EA Act.
7.	Schreiber Fire Department	Comment received November 2021: <ul style="list-style-type: none"> Concern with no new pull off location on proposed realignment and potential for vehicles to park along highway to photograph lake views. Request for pull off to be located along realignment. 	Response provided January 2022: <ul style="list-style-type: none"> Existing Selim pull off to be along abandoned highway, pull off along realignment not warranted.
8.	Ministry of Citizenship and Multiculturalism (MCM)	Comment received December 2021: <ul style="list-style-type: none"> Stage 2 archaeological assessment should be completed prior to any ground disturbance. Impacts to potential built heritage resources and cultural heritage landscapes should be evaluated and documented in EA Report. 	Response provided January 2022: <ul style="list-style-type: none"> Stage 2 archaeological assessment will be undertaken prior to ground disturbance. Screening for cultural landscapes and built heritage including checklists were completed for the project.
Property Owners			
9.	Property Owner	<ul style="list-style-type: none"> Requested hard copy of TESR 	<ul style="list-style-type: none"> Hard copy provided.

APPENDIX B.2 Correspondence in response to the TESR Public Comment Period (2021/2022)

10.	Property Owner	<p>Comments received October and November 2021:</p> <ul style="list-style-type: none"> Concerns with Lakeshore Road intersection and requested modifications to pavement markings to identify left turn slip around and added signage. Proposed alignment would increase noise for residents along Lakeshore Road, preferred to have highway north of hydro corridor. 	<p>Response provided January 2022:</p> <ul style="list-style-type: none"> Left turn slip around was reviewed but not warranted at Lakeshore Road. Noise impact assessment completed which found preferred alignment would not result in distinguishable noise increases. Preferred alternative selected based on numerous factors.
11.	Property Owner	<p>Comment received November 2021:</p> <ul style="list-style-type: none"> Noted tailings north of CP tracks within proposed alignment C, project provides potential for remediation. Impacts to residents from realignment not detailed in report. Noted preference for Option A. Blasting not a concern during construction. Existing highway not high risk for accidents. Reuse existing highway corridor through the park. CP access should be improved to accommodate emergency vehicles. 	<p>Response provided January 2022:</p> <ul style="list-style-type: none"> Appreciated notice of tailings, noted MINES identified potential for fugitive tailings along proposed right-of-way. A plan will be developed if tailings present within proposed right of way. Many factors considered in development of preferred realignment plan. CP access is an internal private crossing not to be used by public. CP no longer requires this access.
12.	Property Owner	<p>Comments received October 2021:</p> <ul style="list-style-type: none"> Questioned east bound lane remaining as passing lane. Requested access be maintained from Lakeshore Road to new Park entrance road to facilitate ski trail grooming and equipment. Requested turning lane onto Lakeshore Road as speed limits will increase with proposed realignment. 	<p>Response provided January 2022:</p> <ul style="list-style-type: none"> Eastbound passing lane will be included in selected realignment. Entrance to the new park road from south of the highway will require an entrance permit and official trail crossings will require encroachment permits. Turning lanes not warranted at intersection as posted speed to remain as is and intersection does not have a pattern of collisions.

APPENDIX B.2 Correspondence in response to the TESR Public Comment Period (2021/2022)

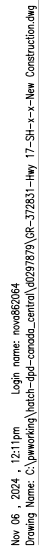
13.	Property Owner	<p>Comment received November 2021:</p> <ul style="list-style-type: none"> Questioned noise levels and threshold of significant, noted existing highway traffic is loud to residents, proposed realignment will increase noise. Questioned campers being a priority over residents. No mention of tailing location in report. No mention of safety when turning off Highway 17 onto Lakeshore Road. 	<p>Response provided January 2022:</p> <ul style="list-style-type: none"> Noise impact assessment completed which found preferred alignment would not result in distinguishable noise increases. Appreciated notice of tailings, noted MINES identified potential for fugitive tailings along proposed right-of-way. A plan will be developed if tailings present within proposed right of way. Turning lanes not warranted at intersection as posted speed to remain as is and intersection does not have a pattern of collisions.
14.	Property Owner	<p>Comment received November 2021:</p> <ul style="list-style-type: none"> Noted disappointment with proposed alignment and lack of direct consultation with property owners. Significant concerns with Increased noise to residents; Impacts to recreational trails and areas north of CP tracks and closure of trails; and Impacts to Whitesand River regarding hiking, fishing, and resident Brook Trout populations. 	<p>Response provided January 2022:</p> <ul style="list-style-type: none"> Foliage does not provide significant noise attenuation unless it is thick, coniferous, and close to receiver or road. The realignment would have less visible shielding from forest. Access to trails requires crossing CP tracks, CP access is an internal private crossing not to be used by public. CP no longer requires this access. Entrance to the new park road from south of the highway will require an entrance permit and official trail crossings will require encroachment permits. Targeted fisheries studies to be undertaken in detail design, water crossings to be designed to meet MTO standards for fish passage requirements. Many factors were considered in selection of preferred alternative.

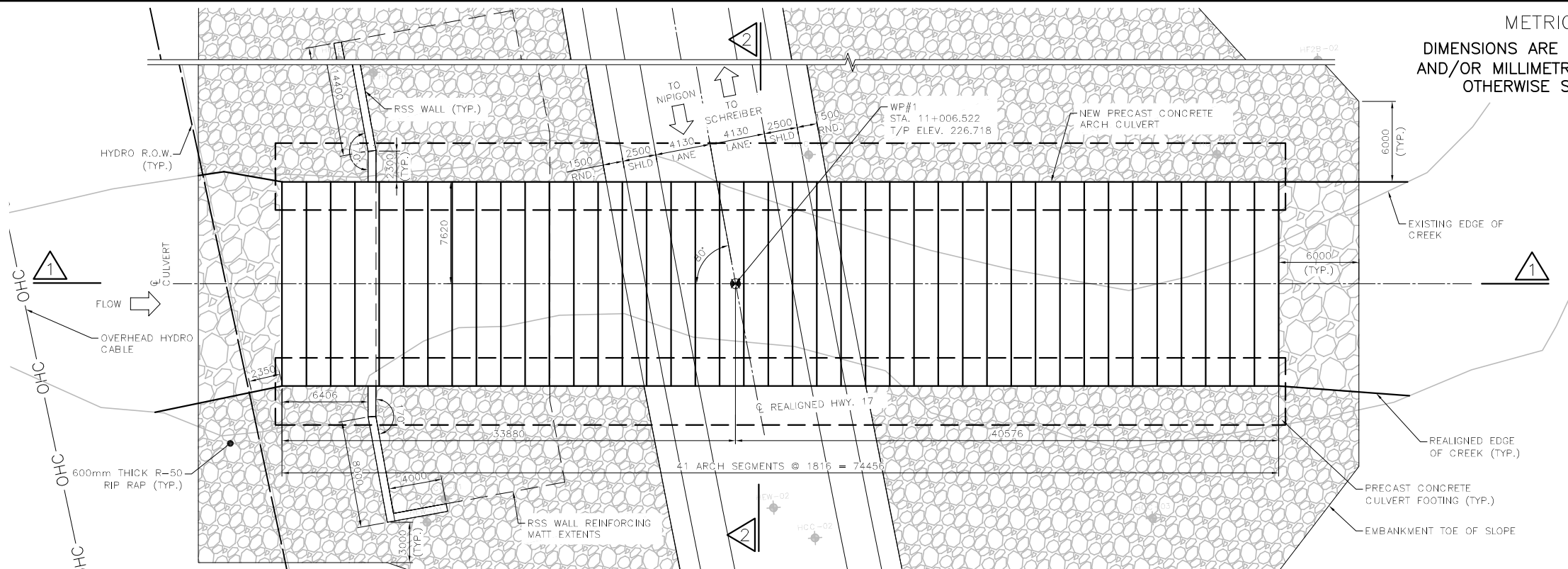
Appendix C

Plans

CONT No. GWP No. 6333-14-00
HWY 17 REALIGNMENT AND HEWITSON CREEK CULVERT NEW CONSTRUCTION

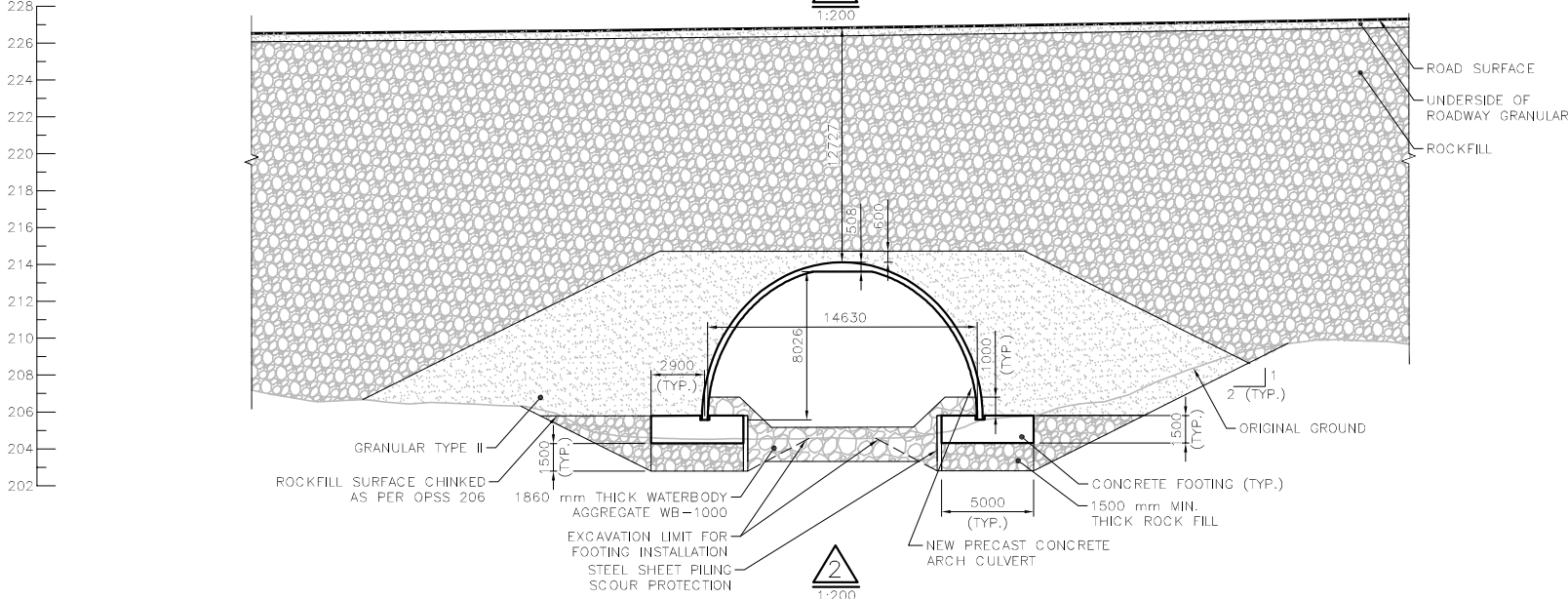
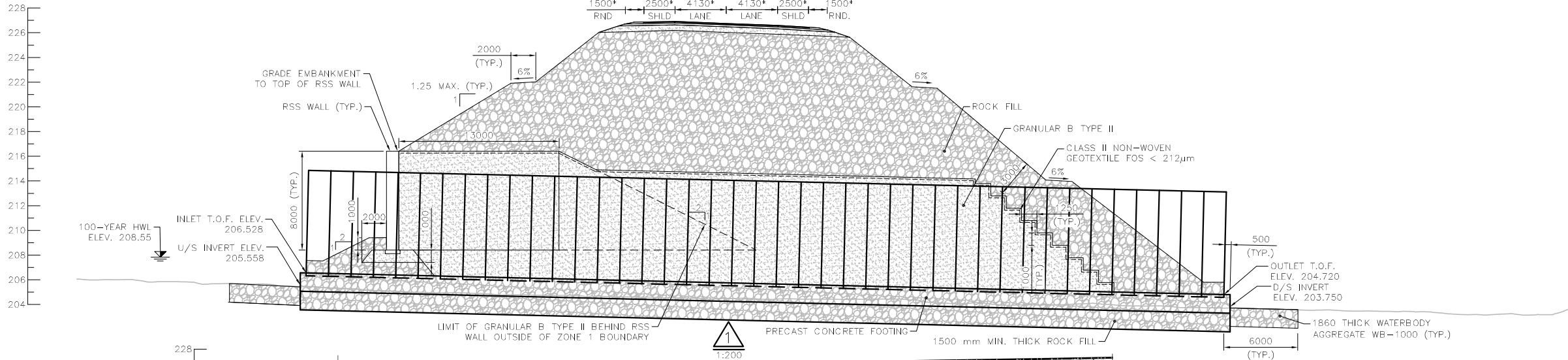
HATCH





PLAN

1 : 200



DRAWING NOT TO BE SCALED
100mm ON ORIGINAL DRAWING

REVISIONS		DATE	REV.	DESCRIPTION
DESIGN	JG	CHK	WB/KK	CODE CAN/CSA S6-14 LOAD CL-625-ONT
DRAWN	AS	CHK	JG	SITE 48C-026/C

CONT No.
WP No.

HEWITSON CREEK CULVERT
REPLACEMENT
GENERAL ARRANGEMENT

HATCH



SHEET
XX